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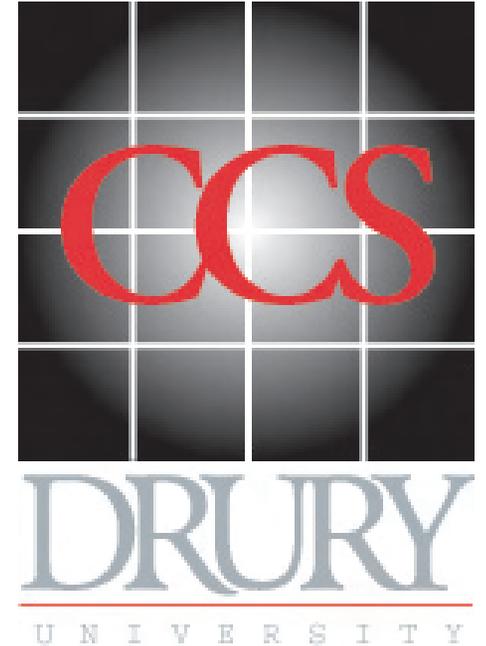
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Sunshine Community Members

For their support and feedback throughout the planning process. The Sunshine community members have helped to make this program such a successful and important educational process.



Introduction

- Table of Contents
- Preface
- Problem Statement
- Mission Statement

Sunshine Street Corridor



Introduction

Table of Contents

Sunshine Street Corridor

Introduction.....	01
Preface.....	03
Problem Statement.....	03
Mission Statement.....	03
Executive Summary.....	04
Character of the Neighborhood.....	05
Street and Streetscape.....	06
Residential Development.....	08
Commercial Development.....	09
Parking.....	10
Zoning.....	11
Phasing.....	12
Final Vision.....	13
Phase 1.....	14
Phase 2.....	16
Phase 3.....	18

Project Research.....20
Case Studies.....21
Goals and Objectives of the Community.....23
Site Analysis.....24
Written and Visual Preference Survey.....29

Visions.....32
Vision 1 by Christine Friederich.....33
Vision 2 by Carly Rickerson.....36
Vision 3 by Kelsey Stein.....39
Vision 4 by Lannette Guerra.....42
Vision 5 by Audrey McNamara.....45

Appendix.....48
Proposed Zoning.....49
Funding Sources.....55
Community Involvement and Resources.....56

Resources and References.....57
Works Cited.....58
Contact Information.....60

Introduction

Preface

Problem Statement

Mission Statement

Preface

As a part of the ongoing endeavor to improve the Springfield Community, a collaborative effort between the City of Springfield Planning and Development Office and the Drury University Center for Community Studies was pursued in order to establish guidelines for the redevelopment of the Sunshine Street Corridor. This redevelopment explores alternative design concepts for the land use, streetscape and street improvements for the half-mile stretch of Sunshine Street between Kimbrough Avenue and National Avenue.

The development of this program took place at the Hammons School of Architecture on Drury University campus during the 2007 spring semester. The project team is composed of one professor and five students: Professor Jay Garrott, Ms. Christine Friederich, Ms. Lannette Guerra, Ms. Audrey McNamara, Ms. Carly Rickerson, and Ms. Kelsey Stein. These students individually developed five visions for the neighborhood that range from very little development which could take place in five to ten years to very invasive development aimed at the next twenty to thirty years from now. These visions are based upon extensive research done through case studies, literary searches, insights from experts and consultants of the City of Springfield and survey results from the Sunshine community members. A final vision was established by the project team to state what they believed to be best recommendations for the redevelopment of the Sunshine Street Corridor. The final vision has been broken down into three phases that occur over a twenty year span of development. The work described in the final vision is to be done gradually and in phases. Each phase can be divided into sub-phases, making the design more economically feasible.

Problem Statement

The City of Springfield Planning and Development brought the Sunshine Street Corridor Visioning project to the attention of the Drury University Center for Community Studies in effort to redevelop the area and explore visions for the future of the Sunshine community. The Sunshine community consists of the residents of the University Heights neighborhood and the Sunshine-Holland Neighborhood. The area of Sunshine Street between National and Kimbrough Avenues is one of the last remaining residential sections of the Sunshine Street corridor. Lined with many nice homes and mature vegetation, the area provides a positive alternative to the commercialization of Sunshine Street. Unfortunately, the physical and economic forces effecting this section of Sunshine Street are causing many area property owners and city planning officials to worry about evolution of this section of the Sunshine Street corridor. The community wishes to explore alternative strategies for the evolution of the Sunshine Street corridor between National and Kimbrough Avenues. This visioning study will investigate alternative design concepts for land use, streetscape and street improvements for this half-mile stretch of Sunshine Street.

Throughout the semester, the project team has learned a vast amount of information about the Sunshine community and its members. Through an immense amount of site analysis, literature search, expert and consultant opinions, and an extensive range of view points and future goals of community members, the team was able to establish many insights, recommendations, and visions for the Sunshine Street corridor. The future of these recommendations and visions is dependent upon the Sunshine community and its leaders to continue the progress and revitalization of this area in the years to come.

Mission Statement

To preserve the residential character of Sunshine Street between National Avenue and Kimbrough Avenue while increasing the economic viability (property value) and maximizing the quality of the neighborhood which includes, but is not limited to, walkability, vegetation, residential scale, and materials.

Executive Summary

The executive summary is a summation of the principle issues and project outcomes for the Sunshine Street Corridor Visioning Project. This summary states issues and concerns that affect this corridor of Sunshine Street and provides recommendations for how each of these issues and concerns might be resolved. The Executive Summary is broken down into seven categories: Character, Street and Streetscape, Residential Development, Commercial Development, Parking, Zoning, and Phasing. A final vision was then established that encompasses all issues and concerns and provides a recommendation for the overall plan of the Sunshine Street Corridor.

Sunshine Street Corridor



Executive Summary

Character

Issues:

How can a residential neighborhood character be recognized along a half mile stretch of a primary arterial corridor that is heavily commercialized on both ends?

How can the residential character be protected and strengthened between the North and the South side of the Sunshine Street?

How can we develop a residential character that does relate to the arterial and strengthens the edges of the neighborhoods to the north and south?

Concerns:

- As a whole, the Sunshine Street neighborhood lacks a unified character and does not resemble a residential neighborhood along a five lane primary arterial street.
- The area has an increasingly large percentage of rental properties.
- Because over 50% percent of the current facilities are rental properties, the neighborhood lacks adequate properties upkeep.
- The neighborhood lacks signage to clearly mark the entrance of the neighborhood.
- The large number of curb cuts and single-family homes fronting Sunshine Street are remnants of the neighborhoods that existed prior to the establishment of the Sunshine Street arterial corridor and no longer work efficiently.
- The Sunshine Street arterial creates a major barrier between the University Heights neighborhood and the Sunshine-Holland neighborhood.
- The residential areas facing Sunshine Street are undergoing significant change due to social and economic pressure.
- The edges of the two neighborhoods facing Sunshine Street no longer relate to the street context. The evolution of Sunshine Street from a small two-lane street into a five-lane arterial has significantly compromised the residential character of the area.

North Side Residential Character



5-lane primary arterial



South Side Residential Character



Recommendations:

The recommendation to improve the character of the neighborhood is to connect the north and the south side of Sunshine Street visually and not physically. A physical connection would not be possible because of the five-lane arterial that separates the neighborhoods. By defining a residential character through architectural style, scale, and materiality, a cohesive visual quality will connect the north and south side of Sunshine Street. Therefore, it is recommended that the Sunshine Street corridor evolve into a more urban residential district. This can be achieved by placing signage at the beginning and end of the corridor, unifying the street lighting, and placing seasonal vegetation along the street and in the vegetated median. This can also be achieved by introducing high-density and mixed-use into the area. Design guidelines will define the residential character of these facilities and specify how the community could evolve into this style of development.

High-Density Housing and Mixed-Use in a Residential Character



Enhancing the residential character through signage and decorative lighting.



Enhancing the residential character through texture and vegetation.



Executive Summary

Street and Streetscape

Issues:

How can a five-lane primary arterial street achieve the character of a neighborhood?

What traffic calming devices can be brought in to make the neighborhood more pedestrian friendly?

Concerns:

- Vehicles driving at forty-five miles per hour do not relate to a residential neighborhood.
- The lack of crosswalks and close proximity of sidewalks to the street creates an unsafe and uncomfortable environment for pedestrians.

No Crosswalks along Sunshine Street



Distance Between Pedestrian and Vehicle



No Sidewalks in the Sunshine-Holland Neighborhood



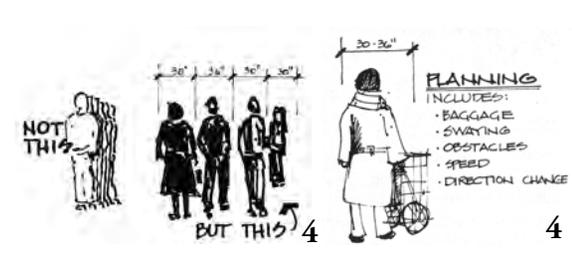
Recommendations:

The recommendation to improve the street and streetscape of Sunshine Street is to establish traffic calming devices that will alert the driver that they have entered a “residential district”. Since Sunshine Street is a primary arterial, it must remain a four lane road with turn lanes. According to the Ozark Transportation Department, a primary arterial is recommended to have a vegetated median and a barrier of some kind separating the pedestrian from the vehicular traffic. The recommendation is to have a vegetated median with turn lanes incorporated at every major four-way intersection. This vegetated median would make drivers aware they are entering a “residential area”, and it has been proven that trees in the median and along the road side psychologically force drivers to decrease their speed. Also, a four foot vegetated buffer is recommended to be placed between the sidewalk and the street. The sidewalks should be improved and widened to six feet. By increasing the width of the sidewalk, pedestrians can walk side by side or pass one another without having to step off the sidewalk into someone’s yard. The four foot vegetated buffer acts as a psychological and physical barrier between the vehicles and pedestrians and promotes a more safe and comfortable pedestrian zone. The incorporation of textured crosswalks, such as brick or stone, will enhance the strong “residential character” by bringing more textures in, and allow drivers to have a visual and physical indication of a pedestrian crossing. In addition, designated sanctuary areas in the median, longer crosswalk times with audible warnings, and proper signage will improve pedestrian’s safety when crossing the street.

Clearly identifying pedestrian crossing through textures and signage.



Proposed Vegetated Buffer Between Pedestrian and Vehicle



Executive Summary

Street and Streetscape

Issue:

How can the number of curb cuts be reduced?

Concerns:

- The vast amount of curb cuts makes it very dangerous for residents to be pulling into or out of their driveways due to the high volume and speed of traffic along Sunshine Street.
- The number of curb cuts make it difficult to introduce the center median.

Recommendations:

According to the Ozarks Transportation Organization recommendations for a primary arterial, residential drives are prohibited along a primary arterial street because they cause a large number of curb cuts. Therefore alleyways should be established. An alleyway implies that each land owner gives up property for the public right-of-way. This may be unacceptable for single family residences, but it would work for multi-family facilities and professional offices. Since the lot sizes are an average of only one hundred-forty feet deep on the north side, and they back up to owned properties, only professional offices that produce a low amount of traffic are recommended. The lots on the south side are an average of one hundred-sixty feet deep and back up to mostly rental properties, so this side can withstand the establishment of multi-family and mixed-use facilities. Increasing the density of lots through the introduction of high-density housing and mixed-use also decreases the number of curb cuts. These buildings only require one or two curb cuts compared to single family homes which would require two to three times as many to house the same number of people. In all established alleyways it is recommended that the power lines be placed underground, the alleyway be made of a pervious surface to reduce storm water run off, and a privacy fence and vegetated barrier be established so properties backing up to the alley way are protected from vehicles traveling through.

Curb cuts in front of single family residences.



Curb cuts in front of high-density housing.



Vegetated Medians protect drivers from using the center median to turn left.



Existing alleyways



Potential alleyways



Issue:

How can the neighborhood be more accommodating to bicyclists and public transportation?

Concerns:

- The lack of bike lanes inhibits the use of bikes on city streets.
- The volume and speed of traffic along Sunshine Street inhibit pedestrians and bicyclists.
- The Sunshine Street corridor is served by a single bus route that seriously limits ridership and city wide access to and from the area.
- The lack of bus turnouts interrupts traffic flow when buses come to a stop.

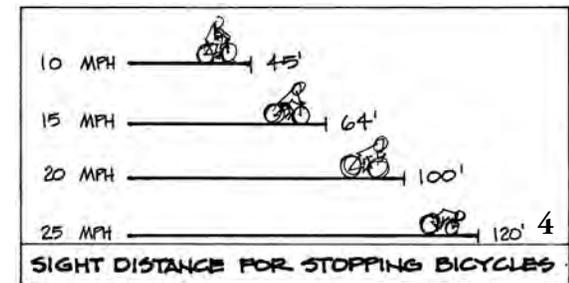
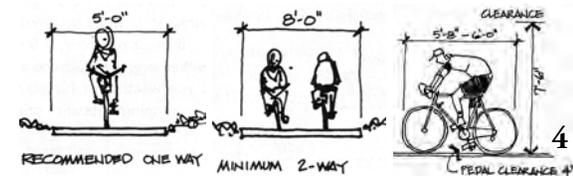
Recommendations:

The expansion of the public transportation routes and the introduction of bike lanes throughout the area would further help to foster sustainable practices among the community and neighborhood residents. We recommend that bicyclists do not use Sunshine Street because it is not safe due to the high traffic volume and speed of the vehicles. Bicyclists are recommended to only cross Sunshine Street from north to south, and use a street to the north or south of Sunshine Street, like University Street, to travel east and west through Springfield. We recommend that the City of Springfield Transportation Department consider the development of bus routes that would serve the north, south, east, and west sides of Springfield and that the frequency of the buses be established at twice an hour to make it more convenient for residents to use. It is further recommended that bus turnouts be placed along Sunshine Street in front of businesses or green spaces so people waiting for the bus are at a safe distance from the street, and buses do not interfere with the flow of traffic when stopped. This placement of bus turnouts is also least likely to affect existing residents. The project team realizes that bus transportation may not be a popular form of transportation right away, but with the ever increasing prices in gasoline and the introduction of more convenient bus routes, the public will change its mind and choose to use public transportation.

No Existing
Bike Lane



Clearly Marked
Bike Lane



Bus Stop along
Sunshine Street



Proposed Bus
Turnout



Executive Summary

Residential Development

Issues:

How will the development of higher-density town homes and mixed-use areas blend together with the existing detached single family residences to make a cohesive residential neighborhood character?

What is the range of housing types (style, height, materiality) the development will offer in order to serve a wide range of residents?

How will the property value of existing homes be effected if higher-density and mixed-use development are in the neighborhood?

Concerns:

- Residents fear that their neighborhood will be weakened by the introduction of high-density housing and mixed-use, because it might appear instead as a commercialized zone with larger buildings, parking lots and intrusive signage.
- Residents fear that bringing in high-density housing and commercial development will decrease their property value and increase crime in the area.
- The percentage of rental homes is continually on the rise, while the upkeep of these homes continue to decline.
- The existing low density single family detached homes, does not respond well to the adjacent primary arterial and its high volume of traffic.
- Many of the properties that are along this section of Sunshine Street were built over fifty years ago and are valued at approximately \$50,000 dollars. Stimulating the conversion of these smaller homes to rental properties and land speculation could lead to a decline in the neighborhood.

Wide Range of Housing Styles along Sunshine Street



Recommendations:

It is recommended that restrictions be placed on the style of architecture, the scale of the building, setbacks, and the type of materials used to assure the compatible coexistence of single-family homes, professional offices and mixed-use facilities within the neighborhood. The existing residential character of the Sunshine community, which includes the University Heights neighborhood and the Sunshine-Holland neighborhood is pitched roofs, tudor or bungalow style houses, one to two stories in height, and made of brick, stone, wood, or vinyl siding. It is recommended that these characteristics be considered when developing design guidelines for the development of higher density housing and commercial establishment within this area. As the homes are replaced, the new construction should be planned and designed to meet the design guidelines and fit into the neighborhood in a sensitive, compatible manner. Strict enforcement of these design guidelines and existing city ordinances related to property ownership and upkeep is needed.

Compact development has been proven to offer greater efficiency and can take advantage of unused capacity of public services and infrastructure. Introducing high-density housing and mixed-use into the area will help provide the infrastructure improvements needed to renew the neighborhood: sidewalks, crosswalks, improvement to the street, street lighting, and vegetation. It will also result in higher density infill residential development which will improve the future economic viability for the neighborhood.



Proposed High-Density Housing in a Residential Style



Executive Summary

Commercial Development

Issues:

This corridor of Sunshine Street is highly sought after by land developers for commercial use because of the declining property conditions and values along this primary arterial street.

What types of commercial facilities should be brought into the community to serve it's wide range of residents (age, income, etc)?

Concerns:

- Residents are currently forced to drive to work, as well as for shopping, dining, and entertainment.
- Commercial buildings must be designed to support businesses that can accommodate a wide range of income levels.
- Commercial uses must be in support of the neighborhood needs.

Recommendations:

It is recommended that small commercial facilities and mixed-use developments be introduced that are conveniently located within a quarter-mile walking distance from the residential areas. Some examples of these facilities that might be located in the area are restaurants, boutiques, bakery/coffee shops, ice cream parlors, a small café, antique shops, or a craft/scrap booking shop. On the north side, we recommend the development of professional offices such as a small doctor or dentist offices and architecture offices and photography studios.

Various Commercial Developments that have a Residential Character



Issues:

What architectural style and restrictions will commercial facilities have to meet within this residential neighborhood?

Concerns:

- Commercial establishments could create improper design and infrastructure responses due to their size, signage, parking and operational requirements.
- Commercial establishments must be designed in a compatible and complementary manner that is respectful of the residential character of the area.

Recommendations:

It is recommended that design guidelines for commercial facilities be developed that are compatible with the residential guidelines. Restrictions should be placed on the scale and materiality of the site and building and on the types of acceptable commercial activities.



Executive Summary

Parking

Issues:

With the development of professional offices, higher density housing, and mixed-use complexes, how can parking be addressed that will fit into the residential character?

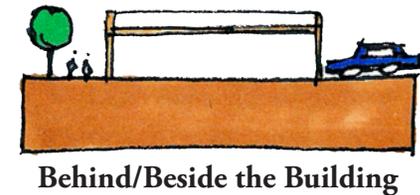
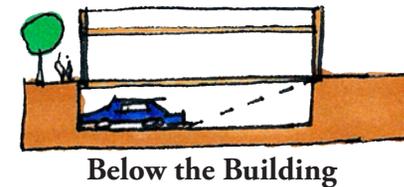
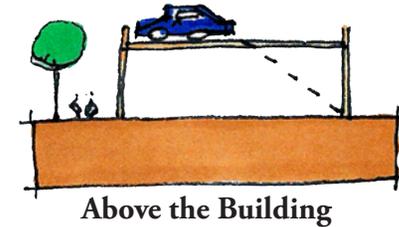
Where should the parking be located and accessed?

What types of parking should be offered and who would be entitled to park there?

Concerns:

- There is no on-street parking or front lot parking allowed along Sunshine Street.
- Parking must occur behind or on the side of the facility.
- Parking buffers must be provided between adjacent properties.

Parking Diagrams by Location



Different Sustainable Parking Surfaces



Recommendations:

North side of Sunshine

The recommendation for parking in the Sunshine community, which includes the University-Heights neighborhood and the Sunshine-Holland neighborhood, is dependent on the location and requirement for each facility. We recommend that all parking occur behind or beside the facility and never on the street or in the front. In the final vision, it is recommended that the north side of the street remain single family detached homes and professional offices in new or converted facilities. Since these lots are not very deep, they cannot withstand a high volume of traffic. The parking for the professional offices would occur behind the facility and be accessed from a side street. The parking lot would be made of pervious materials and would have a privacy fence and strong vegetated barrier to buffer the adjacent residents of the University Heights neighborhood. Mid block curb cuts should be minimized or disallowed.

South side of Sunshine

In the final vision it is recommended that the south side of the street have less single-family detached homes and professional offices, and more high-density housing and mixed-use facilities. In order to increase the economic viability of the area it is recommended that the high-density housing and mixed-use facilities extend south from Sunshine Street. By extending south into the Sunshine-Holland neighborhood, more high-density townhomes could be built along the street edge with rear lot parking. Each townhome would have two assigned parking spots and there would be one and a half parking spots for every 1,000 sq. feet of commercial development in the mixed-use area.

Small Pervious Parking Lot with a Strong Vegetated Barrier



Large Vegetated Parking Lot



Executive Summary

Existing Zoning

The properties along Sunshine Street between National Avenue and Kimbrough Avenue are comprised of 3 different zoning districts. This section of Sunshine Street is predominately single family residential (R-SF) homes. There is one apartment complex, which is zoned High Density Multi-Family Residential (R-HD) and one residence that is zoned as a Planned Development (PD) which is currently occupied.

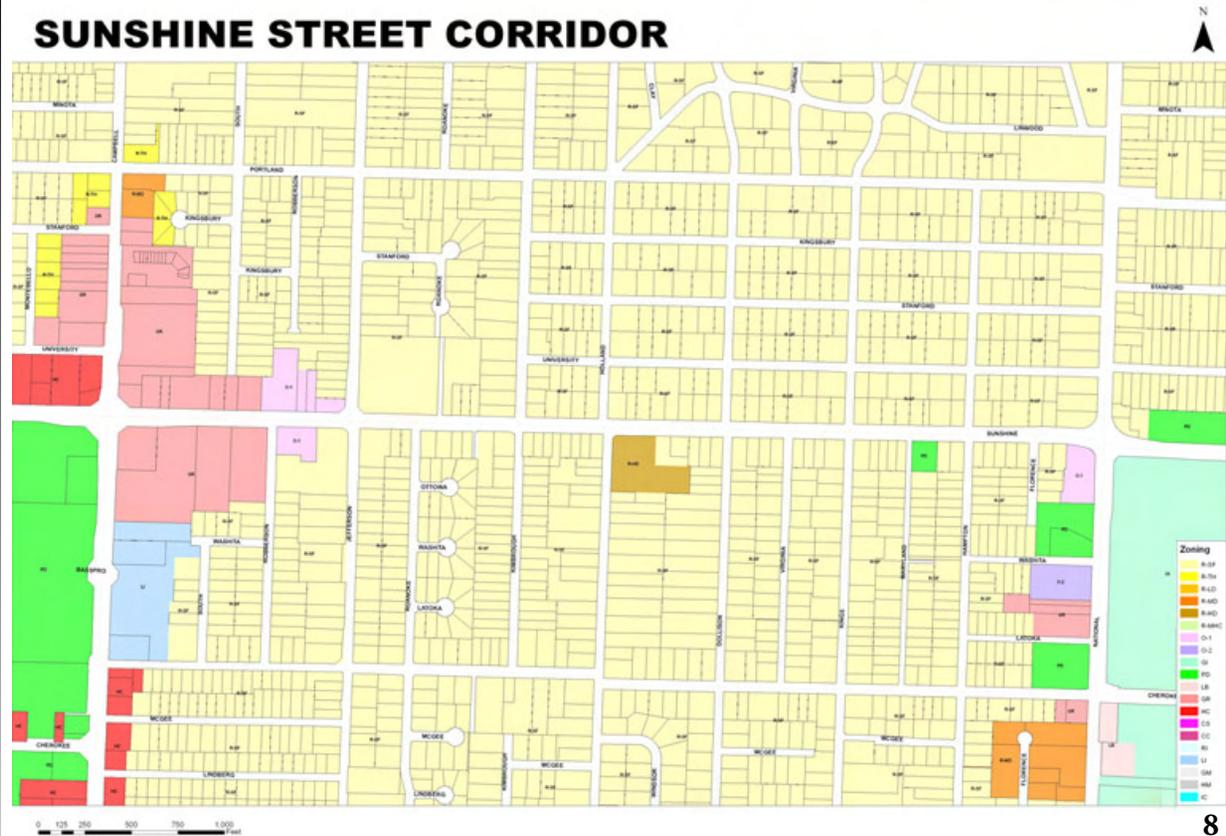
R-SF / Single Family Residential Districts

These are defined by detached dwellings at a low density of approximately 7 units per acre. Certain other structures and uses necessary to serve governmental, educational, religious, recreational and other needs of neighborhood areas are allowed as permitted or conditional uses subject to restrictions intended to preserve and protect the single family residential character of the district. Internal stability, harmony, attractiveness, order and efficiency are encouraged by providing for adequate light, air and open space for dwellings and related facilities and by consideration of the proper functional relationship and arrangement of the different uses permitted in this district.

R-HD / High Density Multi-Family Districts

These are intended to accommodate multi-family developments at densities up to approximately 40 units per acre. It is intended for high-rise apartment development, located in high-intensity use areas specified in the comprehensive plan. This district is intended for areas that have access for vehicular traffic from collector or higher classification streets without traversing minor streets in adjoining residential neighborhoods.

SUNSHINE STREET CORRIDOR



Commercial vs. Residential Development



Commercial Development=Red

Residential Development =Yellow 1

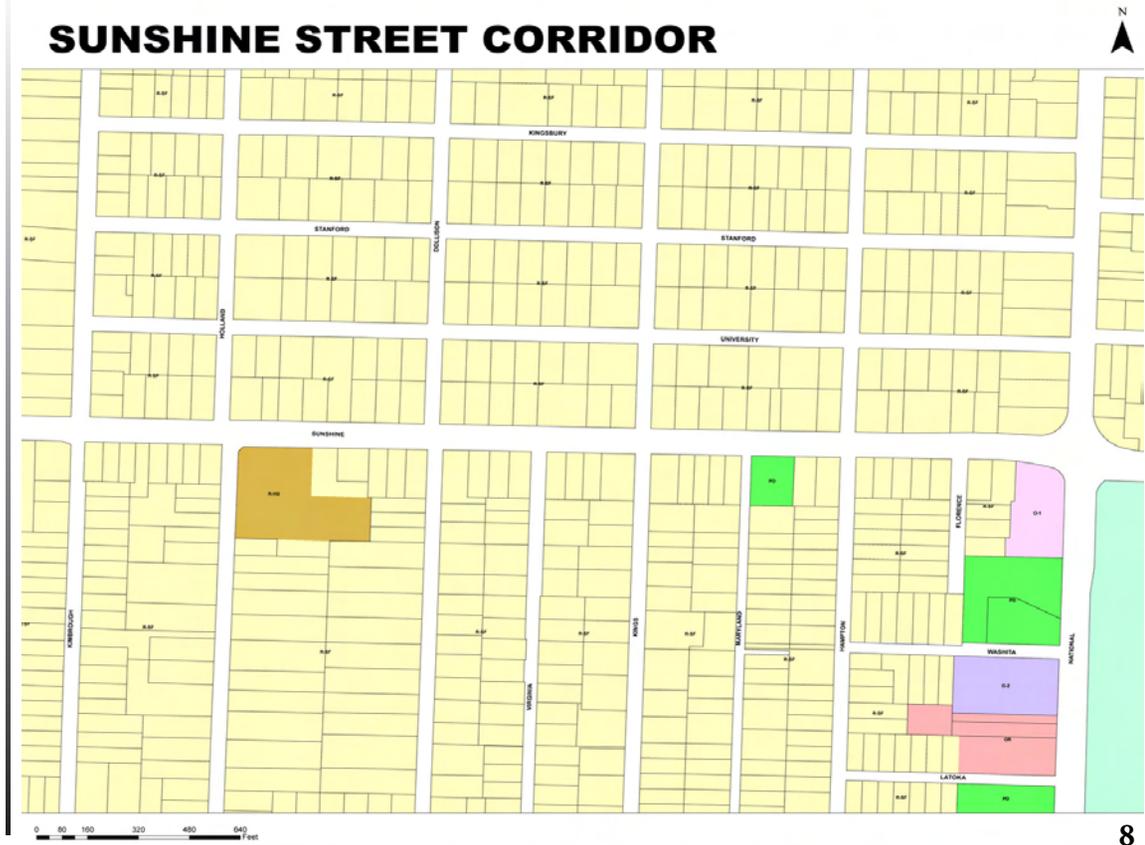
PD Planned Development Districts

These are intended to encourage more creative and imaginative design than generally is possible under conventional zoning regulations. Suitability of such tracts for the PD district designation shall be determined by and made in accordance with the comprehensive plan and designed to lessen congestion in the streets, to promote health and the general welfare, to provide adequate light and air, to preserve features of historical significance, to facilitate the adequate provision of transportation, water, sewerage, schools, parks, other public requirements, and with a reasonable consideration being given to among other things, the character of the district and its peculiar suitability for particular uses and with a view to conserving the land throughout the city.

Design and Development Standards

These provide requirements and guidelines for non structure related site issues such as parking, screening, landscaping, & buffer yards.

SUNSHINE STREET CORRIDOR



Issues & Concerns

As you can see from the existing zoning districts and that of Sunshine Street, to the east and west of this residential area, there is concern about the encroachment of strip mall type commercial developments that have gradually taken over the residential properties along Sunshine Street in the past 50 years.

The existing street classification of Sunshine Street as a 'primary arterial' is not conducive to the low density use of the adjacent single family residences. However there is a strong desire to maintain the existing residential neighborhoods to the north and south of Sunshine Street between Kimbrough Avenue and National Avenue and with that the character of the existing homes along the street.

Executive Summary

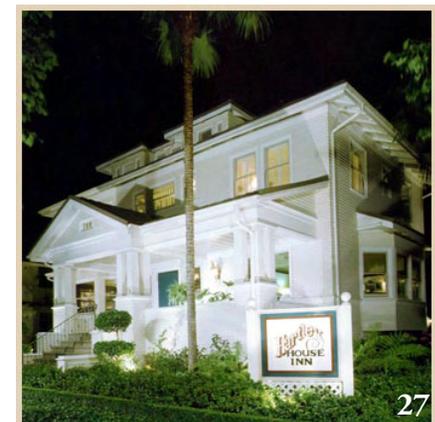
Phasing

Issues:

How can the redevelopment of the corridor be achieved most effectively over time?

Concerns:

- The overall vision of the Sunshine Street Corridor cannot be completed immediately.
- A community identity will be a direct reflection of the redevelopment in the area. Improving the streetscape by adding a vegetated median, widening the sidewalk, placing a vegetated barrier between the pedestrian and the vehicle, and providing crosswalks will enhance the overall character of the neighborhood. Further developing the land use by providing green spaces, community gathering spaces, high-density town homes, and mixed-use facilities will provide residents with a stronger sense of community and a more economically viable neighborhood.



Recommendations:

Stages of Development:

1. *Enforce current laws regarding parking and building maintenance*
2. *Implement an overlay district and design guidelines regarding setbacks, parking, converting residential to commercial, high-density housing, mixed-use facilities, and connection to street so new construction will follow these policies.*
3. *Begin streetscape improvements to: sidewalks, trees, lighting, bike lane, bus turnouts, painting crosswalks, bury electric lines, construct a vegetated median with turn lanes at major intersections and neighborhood signage.*
4. *Promote the development of community gathering/green spaces/community garden*
5. *Upgrade alleyways to enhance usability*
6. *Restore and/or convert existing residential homes whenever possible to professional offices with small scale signage and add small pervious surface parking lot to the rear.*
7. *Develop owner occupied townhomes and mixed-use facilities*



Final Vision

The project challenge was to maximize the economic viability of the area (increasing property value) while still maintaining a residential character. Our vision increases property value while maintaining this residential character by incorporating professional offices, mixed-use developments, and high density housing. The property value is going to be increased by beautifying the streetscape. This would be done through a number of improvements to crosswalks, sidewalks, lighting, the median, and bus and bike access.

Sunshine Street Corridor



Executive Summary

Final Vision

Phase 1 (5 years)



Site Plan of Sunshine Street Corridor



Section of Sunshine Street A-A

Concept for Final Vision-Phase 1

The phasing of the final vision will take place over a period of twenty years. The first phase is planned to be completed in the first five years. These changes will be minimal and will require less investment than later stages. Changes to the property include, converting existing open lots to green spaces and converting existing rental homes to professional offices. Streetscape beautification includes increased vegetation; widened sidewalks and separating them from the curb, and adding bus turnouts.



**View from South Hampton
onto Sunshine Street Corridor**

North Elevation



In the first phase, the changes to the north side of Sunshine Street include increasing the vegetation by replacing the trees lost in the ice storm, and adding new trees and flowers to the existing open lots to convert them to public green spaces. Also, the existing sidewalks should be widened to six feet and a bus turnout should be placed in front of the newly established community green spaces if the city bus routes are expanded. Lastly, the depreciating rental homes should be converted to professional offices with small pervious parking lots behind the facility. These lots should be accessed by a side street or the establishment of an alleyway, and be heavily buffered to protect residents whose homes back up to these converted commercial facilities.

South Elevation



In the first phase, the changes to the south side of Sunshine Street should reflect the north side. The trees should be replanted, and the sidewalks should be widened to six feet. Also, there should be a four foot vegetated barrier placed between the sidewalk and the street. This will require construction along the street curb, and it is recommended that a bus turnout lane be established in front of the existing apartment complex. Due to the size and condition of the houses as well as the nature of the site, many of these homes cannot easily be converted into professional offices. It is recommended that higher density development take place within the next phases.

Executive Summary

Final Vision

Phase 1 (5 years)

Strengths of Final Vision-Phase 1

Green Space

Converting existing open lots to green spaces is an easy way to provide community gathering spaces with relatively little capital investment. These spaces could contain gazebos or even a vegetable or flower garden. The community could choose to take on the task of maintaining these common spaces or a company could be hired to oversee these communal areas. A neighborhood community organization could collect fees to pay this company or the city could maintain it if enough taxes are raised to cover the maintenance costs.



Professional Offices

Converting existing rental properties to professional offices would also increase the property value of the neighborhood with relatively little capital. Professional offices would be possible on the north or the south side of the street because of the low volume of traffic associated with professional offices. Restrictions would be put in place to ensure these professional offices maintain a residential character. Examples of these characteristics include, but are not limited to, scale, materials, setbacks, vegetation, walkability, and types of use.



Streetscape Improvements

Changes would also occur to the streetscape. These include increasing vegetation, widening the sidewalk to six feet, adding a four foot strip of green grass between the sidewalk and the street and adding bus turnouts in front of green spaces. All of these street beautification techniques will raise the property value of the entire neighborhood. Widening the sidewalk to six feet and separating the pedestrian from vehicular zones are recommended guiding design principles established by the Ozark Transportation Organization. Adding bus turnouts to green spaces will increase the use of public transportation and decrease traffic congestion along Sunshine Street. This is desperately needed because of the high volume and speeds of traffic along Sunshine Street. The corner of National Avenue and Sunshine Street is classified, F, the worst classification available for traffic incidents. Bus turnouts would ease this congestion and prepare the community for the next phase of the vision which includes high density housing and mixed-use development.



Weaknesses

The main issues with phase one is the limited development of the vision. This phase of the vision allows for the greatest possibility of negative types of commercial development (strip malls, gas stations, etc.) to be developed along Sunshine Street. Developers have already begun building a strip mall on the northeast corner of Sunshine Street and National Avenue and have attempted to buy up four lots on the northwest corner of Sunshine Street and Kimbrough Avenue. Although this second development was not approved, it, or others similar to it, could be approved in the future.



Negative Examples of Commercial Development

Recommendations

During this time the community must form an organization dedicated to the problem of halting negative commercial development on Sunshine Street. This organization must alert the rest of the neighborhood to this problem in hopes of being proactive as opposed to reactive. If guidelines can be established, in the first five years, that limit types of commercial development along Sunshine Street. The chances of this negative commercial encroachment developing between Kimbrough and National Avenues will be lessened. Community involvement is the greatest weapon to combat negative commercial encroachment. The highest priority for phase one is the establishment of an overlay district that stipulates the planning and design guidelines for this section of Sunshine. (Please refer to the Appendix for the recommended zoning restrictions.)



View from North Hampton onto Sunshine Street Corridor



Positive Examples of Commercial Development

Executive Summary

Final Vision

Phase 2 (5-10 years)



Site Plan of Sunshine Street Corridor



Section of Sunshine Street B-B

Concept for Final Vision-Phase 2

This phase should be implemented within five to ten years and would include high density housing and small scale mixed-use. Streetscape and sidewalk improvements from the first vision should also be included.



View from North Hampton onto Sunshine Street Corridor

North Elevation



In the second phase, the changes to the north side of Sunshine Street include continuing to renovate existing homes into professional offices. If these homes are not feasible for conversion, new construction may occur in their place. This new construction could be professional offices or owner occupied townhouses, but the facilities are required to fall under the design guidelines of scale and materiality to keep within the existing residential style.

South Elevation



In the second phase, the changes to the south side of Sunshine Street include leaving the homes adjacent to Sunshine Street alone and adding new construction of high-density housing behind them. Also, there would be a greater development of green space in open lots.

Executive Summary

Final Vision

Phase 2 (5-10 years)

Strengths of Final Vision-Phase 2

Professional Offices

Residential properties would continue to be converted into professional offices, or removed to allow for the construction of new professional offices. These new professional offices should maintain a residential style and blend in with the existing neighborhood. On the south side of the street, the existing apartment complex would be renovated. This renovation would beautify the complex and raise rental rates. With the higher rents, companies could be hired to manage the landscaping of the apartment complex, ensuring the quality of the complex will be maintained.



Townhouses and Mixed-Use

Also on the south side of the street, townhouses and mixed-use buildings could be created. These larger buildings would take the place of multiple, smaller, single family residences. These buildings would increase density and provide housing for the same number or more of occupants while also providing community green space. Both building types should be done in a residential style and be maintained by companies, ensuring the quality of the lots remains high. Restrictions could be placed on the type of use available in the mixed-use buildings. These buildings, if properly maintained, would greatly increase the property value of the neighborhood. Higher density would also decrease the number of curb cuts along Sunshine Street, which is classified as a primary arterial. According to the Ozark Transportation Organization, no curb cuts should be allowed on primary arterials.



Streetscape Improvements

Street beautification would also continue to take place. The decreased curb cuts would allow for a vegetated median. This would decrease speeds along Sunshine Street and increase the property value of the neighborhood. The four foot strip of grass between the pedestrian and vehicular areas could be improved. Trees or landscaping could be added as a protective barrier between the pedestrians and the vehicles. Textured crosswalks could also increase the safety of pedestrians along the street and tie into plazas or other textured gathering spaces in mixed-use areas.



Weaknesses of Final Vision-Phase 2

Problems associated with phase two of the vision deal with community involvement/ opposition. Unless the community is proactive these changes will not take place and the threat of commercial encroachment will still exist.



1



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View from South Dollison onto Sunshine Street Corridor

Recommendations for Final Vision-Phase 2

Community members should establish an organization to deal with the phasing of the vision. The city should develop Design Guidelines for this section of Sunshine Street in collaboration with the community organization. The greater input and precision in writing the guidelines, the more predictable the outcome and the greater the community satisfaction. Community involvement and proactivity is the greatest contributing factor for community satisfaction in this vision.



1

Executive Summary

Final Vision

Phase 3 (10-20 years)



Site Plan of Sunshine Street Corridor



Section of Sunshine Street A-A

Concept

This is the most invasive phase of the vision and would occur over the next 10-20 years. It includes increasing mixed-use to two or three stories and widening the sidewalks in front of the store to bring them closer to the street. Small cafes, bakeries, and boutiques could be added and outdoor seating areas could attach to them. Further renovation would take place on the apartment complex and more townhomes would be built. An extended-stay community for the hospital could even be built. Sustainable strategies should continue to be implemented.



View from south Hampton onto Sunshine Street Corridor

North Elevation



In the third phase, the changes to the north side of Sunshine Street include continuing renovation of the existing homes into professional offices and increasing the percentage of owner occupied townhomes. This new construction could be professional offices or owner occupied townhouses, the facilities would be required to fall under the design guidelines of scale and materiality to keep within the existing residential style.

South Elevation



In the third phase, the changes to the south side of Sunshine Street include introducing high-density and mixed-use housing along Sunshine Street. There should be a total of 70% high-density and mixed-use and 30% single family residential homes. The only rental properties along Sunshine should be high-density housing with one block property owners.

Executive Summary

Final Vision

Phase 3 (10-20 years)

Strengths of Final Vision-Phase 3

Mixed-Use

Uses in mixed-use buildings could be expanded to include cafes, bakeries, and other small shops. Outdoor seating areas for these restaurants would be on side streets or on the backside of buildings facing Sunshine Street; not directly on the street. Widening the sidewalks to bring the buildings closer to the street creates a larger walking area and increases walkability. Adding seating, lighting, and vegetation also enhances the pedestrian area. Residents of the extended-stay community would support the small businesses.

Extended Stay Community

The extended stay community would be located within walking distance or driving distance from the hospital. This would allow people from surrounding communities to stay near their loved ones who are in the hospital for a short stay. Green spaces, outdoor cafes, and small plazas would provide relaxing places for these guests. The small cafes would provide them with breakfast or lunch for themselves or a snack to brighten the day of their loved one in the hospital. These extended-stay communities would also be maintained by companies and increase property value of the neighborhood.

Sustainable Strategies

A sustainable strategy that could be incorporated might include pervious paving. This type of paving in the apartment complex, townhouse, mixed-use, or extended stay housing would give a greater residential character to these large complexes than traditional paving. It would also help manage storm water run-off and become a better transition to the single family homes set back from Sunshine Street. Bioswales in the vegetated median or vegetative barrier between the street and the pedestrian areas could also help manage storm water runoff. Photovoltaics (solar panels) could be installed on the south side of the roof on businesses to increase energy savings and property value. Solar powered lighting could also be installed along the street. Making the street more pedestrian-friendly and increasing the use of public transport are also sustainable strategies.



Weaknesses of Final Vision-Phase 3

This is the most invasive phase of the vision. If proper prior steps are not implemented and if proper design guidelines are not established, these high-density developments may not achieve the intended residential style. There may also be great resistance from the community to the implementation of this phase.



Buildings not in a Residential Style

Recommendations for Final Vision-Phase 3

Establishing design guidelines for maintaining a residential character in these commercial developments is key to the success of this phase. Open-mindedness and foresight is also necessary. This vision will drastically change the way Sunshine Street is used; but change the visual character of the street very little. This vision is meant to improve the overall quality of the street.



View from north Hampton onto Sunshine Street Corridor



Example of High-Density Housing in a Residential Character

Project Research

- Case Studies
- Goals and Objectives
- Site Analysis
- Surveys

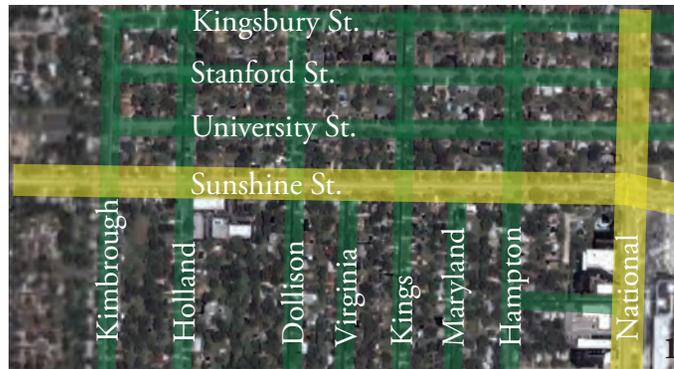
Sunshine Street Corridor



Project Research

Case Studies-Brookside, Kansas

Comparison Between Sunshine Street and Brookside



■ 4 Lane Roads
■ 2 Lane Roads

Sunshine Street



■ 4 Lane Roads
■ 2 Lane Roads

Brookside Blvd.

The Shops at Brookside



Brookside, planned and built in 1920, was Kansas City's first suburban shopping area. It is home to more than seventy unique shops, restaurants, and offices. All are in keeping with the original neighborhood charm and architecture. Some shops are converted residential houses, others were designed to blend with the neighborhood. The shops at Brookside are divided into three categories:

- Services
- Shops/Boutiques
- Food/Restaurants

63rd Street (Sunshine Street)

Key Elements

- 30 degree parking
- Wide sidewalk
- Vegetation
- Lighting
- Signage



1. World's Windows



3. Latin American Imports



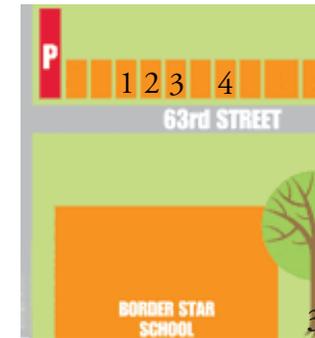
Sidewalk View



2. Brookside Toy & Science



4. Shop Beautiful



63rd Street (Sunshine Street)

Brookside Blvd. (Dollison St.)

1. Joe D's Restaurant

- Parking Lot
- Wide Sidewalk
- Vegetation
- Lighting
- Signage
- Patio/Outdoor Seating
- Umbrellas



Outdoor Seating



Patio with Vegetation

2. Foo's Fabulous Frozen Custard

- 30 degree parking
- Wide sidewalk
- Vegetation
- Lighting
- Signage
- Benches
- Outdoor Seating



Residential Feel



Benches & 30 degree parking

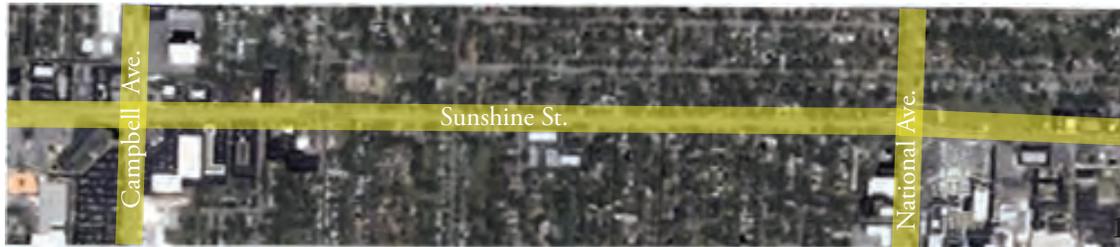


Brookside Blvd. (Dollison St..)

Project Research

Case Studies-Kirkwood, MO

Comparison Between Sunshine St. and Kirkwood Rd.



■ 4 Lane Roads

Kirkwood maintains its residential character through gathering spaces, like small parks with benches and water features. It uses alternative transportation such as bike and bus and is a pedestrian friendly neighborhood. The area incorporates mixed-use and high-density housing and has ordinances concerning signage and lighting to maintain the residential character of the neighborhood. The ordinances also affect the residential homes which have been converted into law offices, doctor offices, and small boutiques. (see below)





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5

Community Activities

Kirkwood is known for its sense of community. These activities strengthen the community. Many of them, such as the Halloween Walk, take place in residential homes, residences that have been converted to commercial, and in mixed-use buildings. Kirkwood community activities are as follows:

- Brunch with the Bunny
- Second Saturday-Music at the Market
- Festival of Food & Flowers
- Summer Concert Series
- Annual Sidewalk Sale
- Dog Days Celebration
- Halloween Walk
- Photos with Santa

Mixed-use buildings also help Kirkwood maintain a sense of community. Parks and fountains create gathering spaces that are vegetated. Outdoor plazas and eating areas in mixed-use buildings are man-made examples of gathering spaces that help build a sense of place. Pedestrian friendly sidewalks make travel between these gathering spaces easy.

Project Research

Goals and Objectives

Introduction

Three meetings were held during the course of this project, one was with the City of Springfield and two were with residents of the Sunshine-Holland and University Heights neighborhoods. During the first community meeting the values (listed below) of the residents and users of Sunshine Street were derived from the questionnaires and surveys given to them and through site analysis conducted by the team. The values are divided according to four categories developed by William Pena: form, function, economy, and time. They are also categorized in order of importance. The project goals and objectives were then derived from the community's values. After we determined the goals and objectives of the project we developed three synthesis statements that highlighted the most important goals of the project. These synthesis statements guided the visioning process.

From these synthesis statements the team produced five visions which they presented to the City of Springfield during an in-class critique. From this critique the team further refined their visions and presented them to the community during their second meeting. From the comments and insights gained during this meetings, the team proposed a final comprehensive vision for the Sunshine Street corridor.

Values

1. Form:

- The residential character of the neighborhood including safety, walkability, vegetation, materials, and human scale
- The quality of the neighborhood

2. Economy:

- Make most of existing land use (ex. high-density housing)
- Practicality and economy in design choices (proper lighting, signage, etc.)

3. Function:

- Sustainability including correct site response, vegetation, and various modes of transportation (pedestrian, bike, bus)

4. Time:

- Long range economic viability of the area



Goals and Objectives

1. Form

- Vegetated median to create a green canopy and decrease traffic speeds
- Bike lanes
- Wide sidewalks with vegetative barriers to protect pedestrians
- Renovate the existing buildings on the north side with professional offices, restaurants, etc. and create small strip centers in a residential style on the south side that will house the same amenities
- Appropriate signage

2. Economy

- A combination pedestrian and vehicular streetlight, solar powered and within budget

3. Function

- Mixed use buildings because of the road classification and situation
- Bus turnout to ease traffic congestion and increase sustainability
- Refuge island and textured crosswalks, textures alone are not enough to keep pedestrians safe
- Townhouse apartments and other forms of high-density housing

4. Time

- Through programming and design, prevent chain restaurants and strip malls, from encroaching into the neighborhood.

Synthesis Statements

- Prevent the encroachment of commercial development not in a residential character (materials, scale, vegetation, style, etc.).
- Develop a stronger sense of community in the existing University Heights and Sunshine-Holland neighborhoods.
- Maintain the residential character of the neighborhood, which includes, but is not limited to materials, scale, vegetation, and style.
- Making a more sustainable community by increasing vegetation, walkability, public transportation, high-density housing, mixed-use buildings.



Project Research

Site Analysis

Introduction

The site (Sunshine Street) was analyzed through its qualitative and quantitative aspects. The qualitative aspects describe the feel of the space. The quantitative aspects are factual. The following excerpts written by Kelsey Stein, express the site qualitatively, both on foot and by car.



Qualitative Aspects of the Site

“The speed of the cars is overwhelming. On my left sit idyllic 1930s bungalows and 1950s ranch-style houses with neatly manicured yards. Children should be playing in the yards and new moms should be getting in their “30 Minutes 3 Times a Week” by jogging behind off road baby buggies with oversized tires. But this isn’t happening, because on my right is a five lane super-highway. Sunshine Street. A major artery, without which, the strip mall economy of Springfield would collapse. These few blocks of residential homes are all that remains of Sunshine Street’s beginnings.”



Two Interpretations of Sunshine Street

“By car, the wide street feels warm and inviting. The huge, beautiful trees and historic homes reveal the friendliness of the neighborhood, even at 45 mph. This is an area I wish I could stay in longer, but soon I am driving through barren strip malls and fast food restaurants again.”



“On foot, it is a different story. the homes and trees are just as beautiful, but the wide street, accommodating to the vehicles, encroaches on the sidewalk; my space. A mere two feet separates me from a half ton of steel. I could reach out and grab a Chevy if I wanted to. This is most apparent in the dwindling front yards of these homes. The noise of the cars is deafening.”



Project Research

Site Analysis

Qualitative Aspects of the Site

Pros:

- Friendly Neighborhood
- Mature Vegetation
- Residential Character
- Human Scale



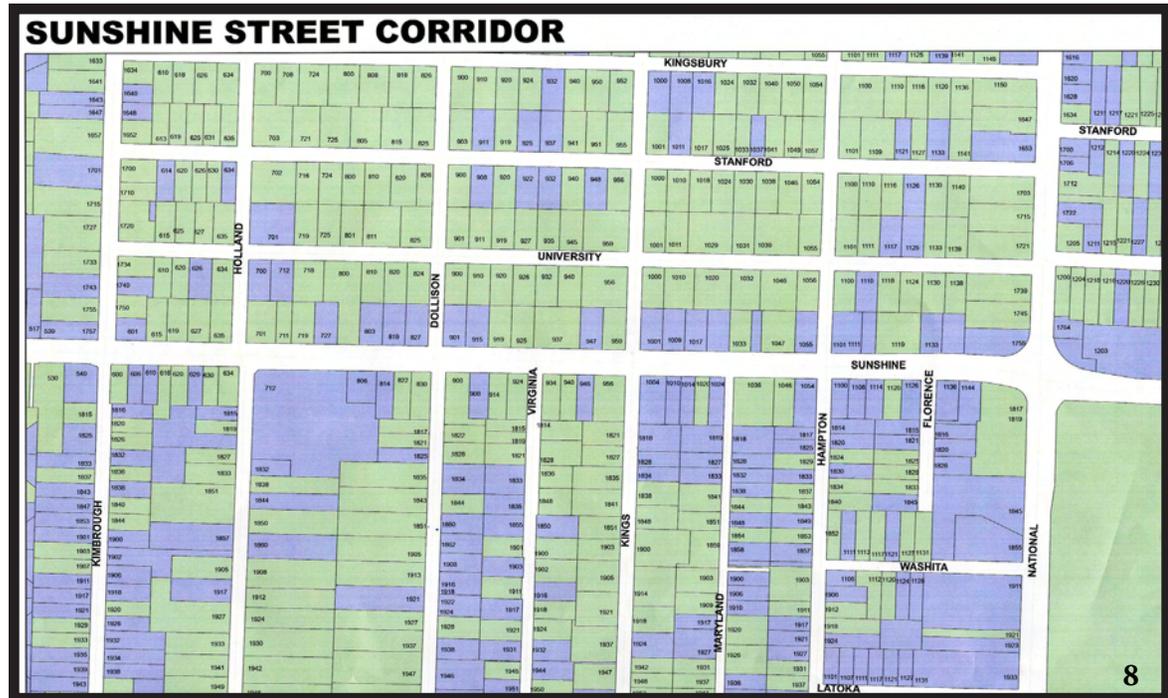
Cons:

- Noisy
- High Traffic Speeds
- Difficult to Cross (pedestrians)
- Difficult to Turn Left (vehicular)



Quantitative Aspects of the Site

The high number of rental houses (blue) to owner houses (green) makes this area susceptible to commercial development. One of our goals is to increase the property value of this segment of the Sunshine Street Corridor, thereby decreasing the area's susceptibility to strip mall commercial development.



Rental vs Owner Properties

The section of Sunshine Street between National and Kimbrough Avenues (see boxed area below) is surrounded by strip mall commercial development to the east and the west. These areas are represented by the color red. The primary residential areas are in yellow.



Commercial Encroachment

Project Research

Site Analysis

Styles of Houses

There are sixty-two homes along the Sunshine Street Corridor that are various styles. (see right) The most popular style is the ranch, which totals to thirty-two. The second highest is the bungalow style at approximately sixteen. Finally, the remaining ten homes along the corridor are in the Colonial, Cape Cod, Old Style, and Tudor styles. Refer to the diagram below to reference the different styles of houses along the Sunshine Street Corridor.

Year Houses Were Built

The houses on Sunshine Street range from 1920 to 2004. Although, no homes were build in the 1980's or 1990's. The majority of homes were built in the 1930s, followed by the 1940s, and then the 1950s. Refer to the diagram below.



Cape Cod (1 story)



Bungalow (1 story)



Tudor (1 story)



Cape Cod (1.5 stories)



Bungalow (1.5 stories)



Tudor (1.5 stories)



Ranch (1 story)



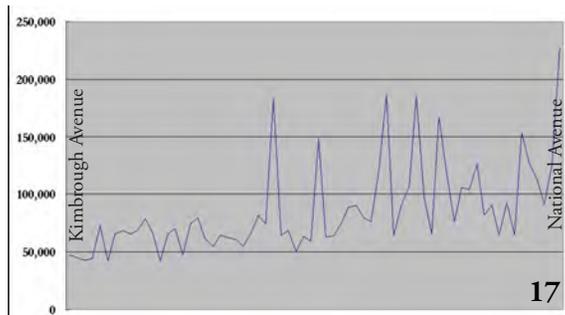
Colonial (1 story)



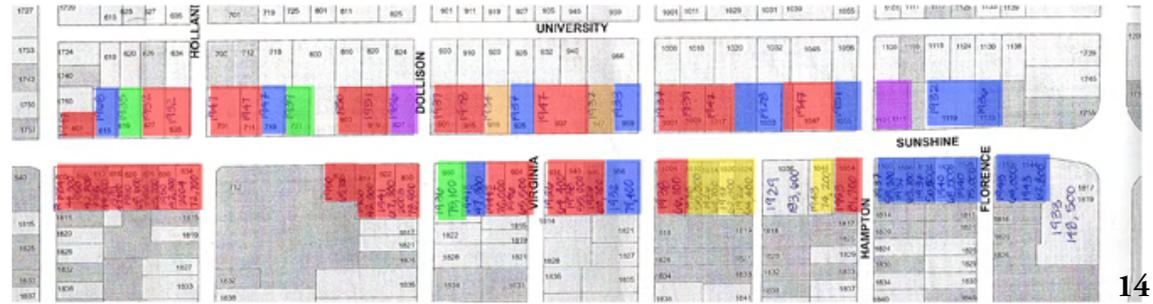
Old Style (2 stories)

Property Value

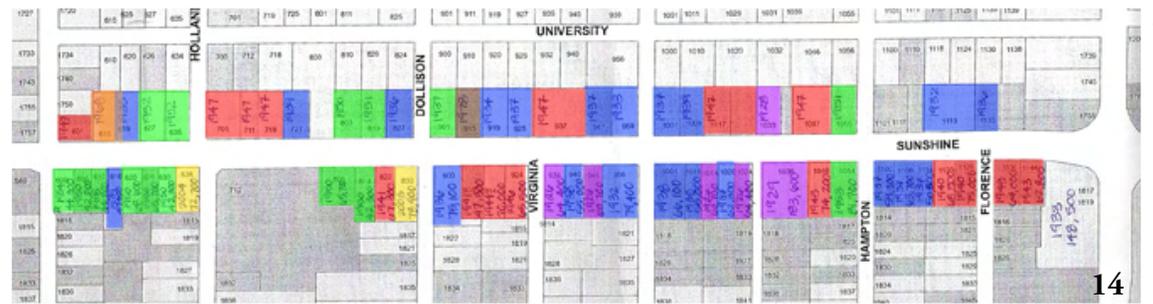
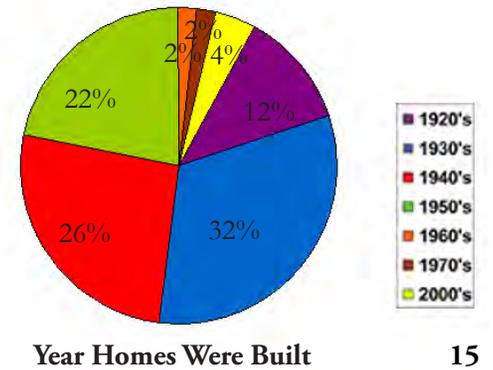
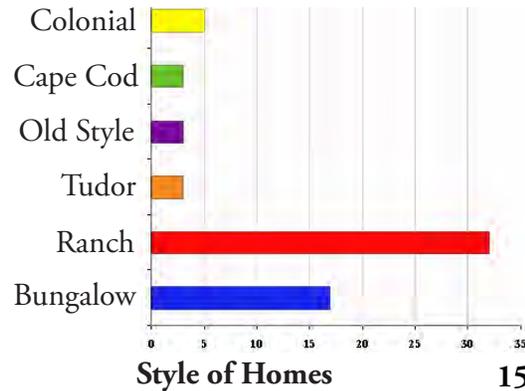
The majority of homes on Sunshine Street range from \$50,000 to \$75,000, with a few homes priced over \$150,000. The age of the structures combined with the low property value of the majority of the homes is a prime target for developers. These developers have attempted to buy multiple adjacent lots with plans to destroy the homes and build strip malls in their place. This would continue the commercial development already taking place on either side of Sunshine Street, west of Kimbrough Avenue or east of National Avenue.



Property Value along the Sunshine Street Corridor 17



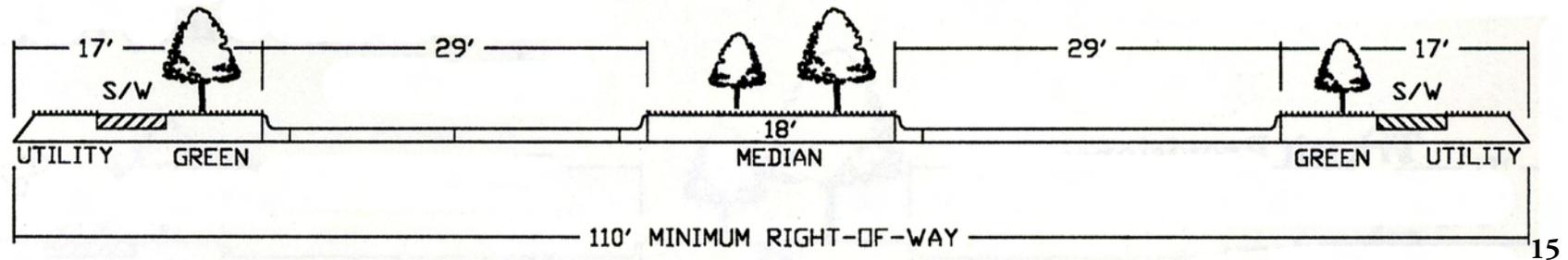
Style of Homes 14



Year Homes Were Built 14

Project Research

Site Analysis



Ozark Transportation Organization Design Guidelines for Primary Arterials

Speed – 35-45 mph

Width of sidewalk – 4-5 ft (minimum) on both sides

Bicycle Routes – Provided according to adopted plan

No residential drives permitted

Primary Arterial

Sunshine Street is designated a primary arterial. The design recommendations for a primary arterial, from the Ozark Transportation Organization, are seen above.

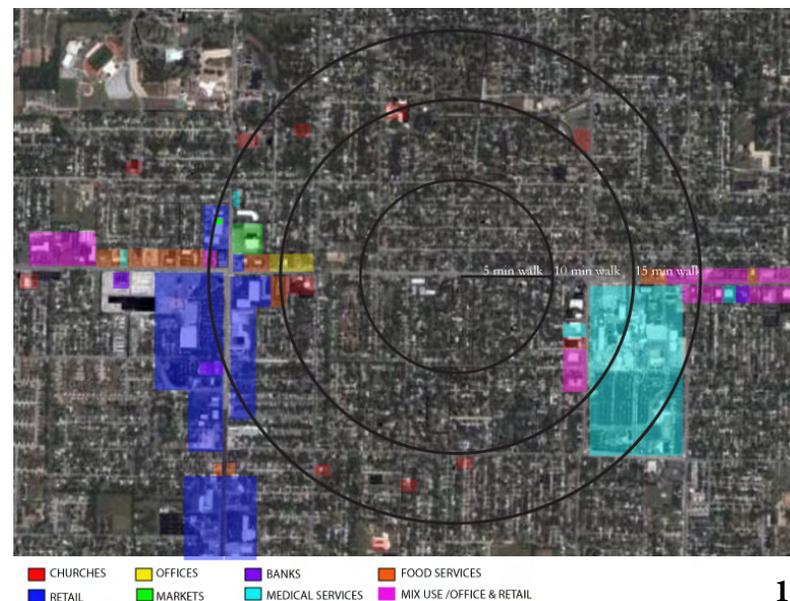
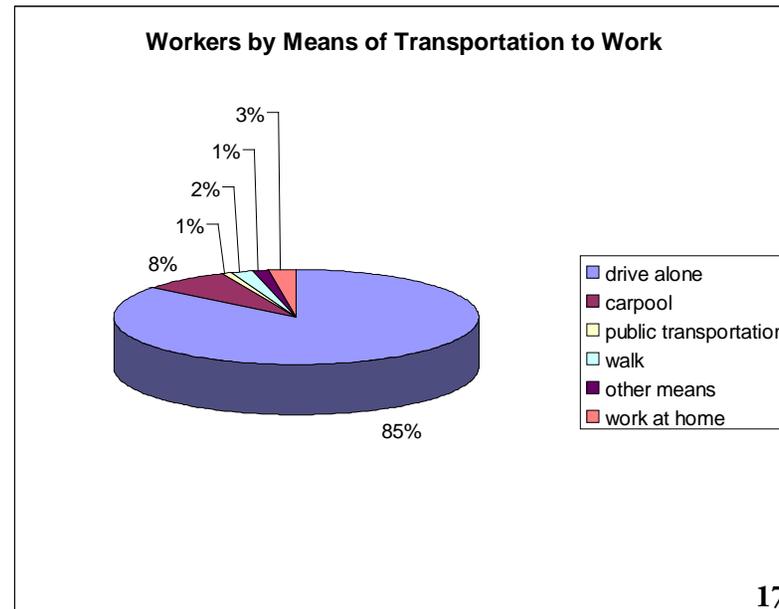
Currently, Sunshine Street does not meet the requirements for width of sidewalk or vegetated barrier separating pedestrian from vehicular traffic. It also does not have a vegetated median or bicycle routes. Finally, Sunshine Street has a multitude of residential drives.



Transportation

The chart on the upper right shows that eighty-five percent of the residents living within a 0.3 mile radius of Sunshine Street between National and Kimbrough Avenues drive to work alone. Only 1% of the residents use public transportation. Sunshine Street has high traffic congestion and a high number of accidents, so by increasing public transportation it would alleviate some of the stress on the street. The average number of vehicles per household is 1.5. This contributes to high traffic volumes because 40% of the households on Sunshine Street have only one resident. Over 50% of residents work 10-19 minutes from home. This means these residents are driving long distances during peak rush hours, adding to traffic congestion. (2000 census, 2005-2010 Springfield Demographics).

The map on the lower right shows walking times from the center of the Sunshine Street Corridor. There are no commercial amenities less than a fifteen minute walk from the University Heights or Sunshine-Holland neighborhoods. Residents will not walk to these commercial areas because they are located too far away and because the walk to reach them is unpleasant.



Project Research

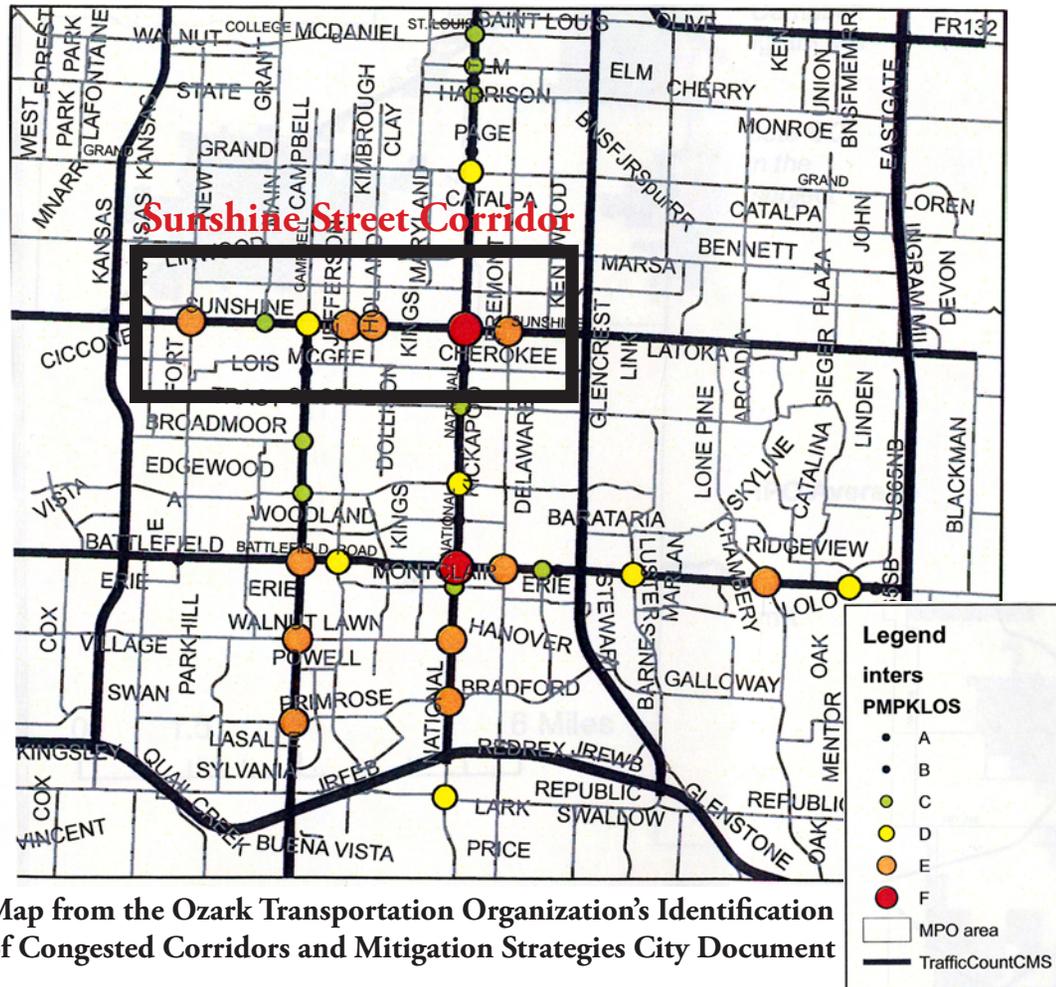
Site Analysis

Dangerous Intersections

The corner of Sunshine Street and National Avenue has the worst rating for vehicle accidents in the city, F. Two more intersections within the Sunshine Street Corridor have the second worst rating, E. Between January 1, 2007 and February 9, 2007 thirteen accidents were reported on this section of Sunshine Street alone (CrimeSeen Website). Increasing public transportation would ease traffic congestion and decrease the number of accidents. Introducing traffic calming devices such as bulb outs and vegetated medians would also help to decrease traffic speeds.

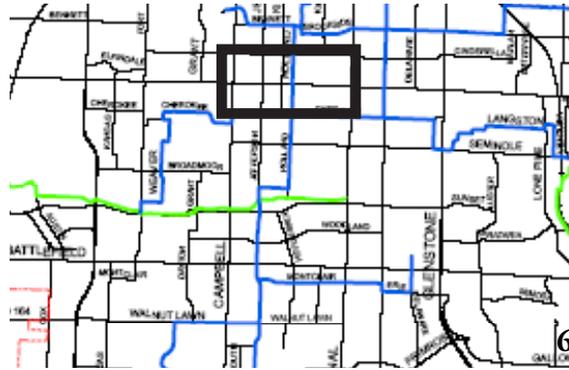


Corner of National Avenue and Sunshine Street



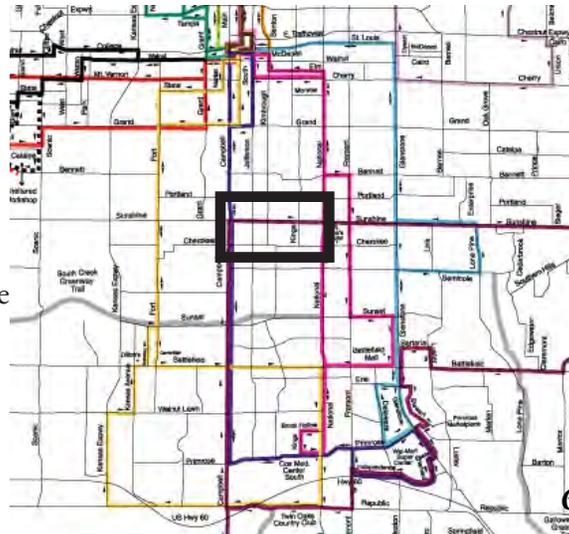
Bike Routes

There are currently no bike routes along Sunshine Street. A bike route runs along Holland Avenue and bisects Sunshine Street. Currently, biking on Sunshine Street is very dangerous due to the high traffic speeds and volumes of traffic. The addition of a bike lane would increase cyclist safety. Other traffic calming devices like a vegetated median, bulb outs, and bus turnouts would also make biking more feasible on Sunshine Street.



Bus Routes

Currently, there is only one bus route on Sunshine Street. It runs a closed loop down Sunshine Street to the mall and the library. It makes stops along Sunshine Street every hour. Increasing bus destinations and the number of times the bus runs along Sunshine Street would increase users and decrease traffic congestion. Bus turnouts could be located in front of mixed use buildings and green spaces.



Project Research

Surveys

A series of surveys were given to the eleven residents present at the first community meeting to help the team determine goals and objectives for the Sunshine Street Corridor Visioning Project. These residents own properties along Sunshine Street between National Avenue and Kimbrough Avenue. After these surveys were given, they were discussed and reviewed thoroughly. Here are the results:

Written Survey Results:

The written survey gave us a better understanding of where community members resided. Eighty percent of the residents that took this survey were from the University Heights neighborhood, which is located on the north side of Sunshine Street. The major issues found through this survey were that seventy percent of people felt that single family residences along Sunshine Street should not remain solely residential and that they would like to see professional offices, antique shops, boutiques, and photography studios brought into the area. Also ninety percent of the community members would like to see more green spaces/park areas along Sunshine Street. Finally, community members commented that they would be in favor of wider sidewalks, electric lines placed underground, traffic calming devices, bike lanes, trees replanted that were lost in the ice storm, and a more pedestrian friendly neighborhood.

Visual Preference Survey Results:

The visual preference survey allowed us to canvas the community members on ten issues that would be effecting their residential neighborhood. These ten issues were medians, sidewalks, crosswalks, street lighting, bicycle and bus lanes, green spaces, high density housing, commercial styles, and signage. The community was presented with four different pictures for each of the ten categories and asked to circle the one they liked the most and put an X through the one they disliked the most. There was space allotted next to each photo to make any additional comments. The results from this survey are listed below and on the following two pages.

Written Survey

Each year the Drury University Hammons School of Architecture works on a community revitalization project. This year, in conjunction with the Springfield Planning and Development Department, we are providing ideas & visions to improve the land use and streetscape of Sunshine Street, between National Ave. and Kimbrough Ave. As a resident of this community, we value your opinions and suggestions. We respect your privacy, so if there are any questions you do not feel comfortable answering, feel free to leave them blank. We will be presenting our ideas to the community throughout the spring semester. Please provide contact information if you would like to be involved in the future. The more feedback we receive the better we can improve your community.

Address: _____

How many members live in your household and what are their ages? _____

Do you rent or own your home? How long have you resided at this residence? _____

What is your profession/occupation? _____

Where do you work? For example, downtown, south or north side of town? _____

Would you use public transportation if it were more accessible or convenient? _____

How many vehicles are at your residence? _____

Do you know your neighbors by name? _____

Do you feel that the single family residences along Sunshine St. should remain solely residential? _____

Would you like to see some businesses/restaurants on Sunshine St. between National and Kimbrough that maintain a residential feel and retain the existing architectural character of the home? If so, what types of businesses would you support in the neighborhood? Restaurants, professional offices, community centers, retail shops, etc. _____

Do you feel that the current street lighting is adequate on Sunshine St.? _____

Would you like to see more green spaces/park areas along Sunshine St.? _____

What changes to your neighborhood would you like to see in the future? For example, pedestrian friendly sidewalks and crosswalks, traffic calming devices, bike lanes, community gardens, community gathering areas for events, farmers markets, etc. _____

Please feel free to write any additional comments concerns, or suggestions on the back of this sheet. Please return this survey in the enclosed envelope. Thank you for your time.

Visual Image Survey

Types of Medians



Results and Conclusions

Types of Medians

Sunshine Street community members voted in favor of the vegetated median as long as it had turn lanes at major cross streets. The vegetated median would act as a traffic calming device by making it safer for pedestrians to cross the street and helped the neighborhood achieve a more residential feel.

Types of Sidewalks

Sunshine Street community members voted in favor of bringing in textured sidewalks with seating to provide a warm, and inviting place to sit; “without having to buy anything.” They voted against the traditional wider sidewalk without the vegetated barrier from cars because it provided a non descript environment and lacked a human scale.

Types of Sidewalks



Project Research

Surveys

Types of Crosswalks



Results and Conclusions

Types of Crosswalks

Sunshine Street community members voted in favor of having textured crosswalks because it alerts the driver of a pedestrian crossing through a change in materiality and texture and better identifies this area's residential atmosphere. They voted against the existing conditions of Sunshine Steet.

Types of Street Lighting

Sunshine Street community members voted in favor of the decorative street lights because they were compatible to the character of the neighborhood and provided lighting at a human scale for pedestrians. They were also in favor of seeing if the solar powered street light could become more decorative to allow their neighborhood to become a more sustainable community. Community members once again voted against the existing standard street lights because they were only at a vehicular scale and provided the neighborhood with no residential character.

Types of Street Lighting



Bicycle Lanes and Bus Stops



Bicycle Lanes and Bus Stops

Sunshine Street community members voted in favor of having bike lanes, but would prefer to have them separated from the pedestrian sidewalk. Their only concern was how much of their property would be taken up if this were to occur. Community members also voted in favor of having bus stops along Sunshine Street to make public transportation more feasible to them. Bus stops would be located off the sidewalk as to not interfere with pedestrian traffic and bus turnouts could be located in front of green spaces or mixed-use facilities so the flow of traffic along Sunshine Street would not be interrupted.

Green Spaces/Park Areas

Sunshine Street community members voted in favor of converting open lots into community green spaces for neighbors to gather, sit, and socialize. These green spaces provide the potential to hold holiday events, a farmers market, and even a community vegetable or flower garden.

Green Space/Park Area



Project Research

Surveys

High Density Housing



Commercial Styles



Results and Conclusions

High Density Housing

Sunshine Street community members voted in favor of owner occupied, medium density townhouses. These townhomes would be designed to fit in with the residential style already occurring on Sunshine Street and would increase land value of the existing run down rental properties. Community members dislike the existing apartment complex because it does not fit into their residential setting and is a “haven” for high crime in their neighborhood.

Commercial Styles

Sunshine Street community members voted in favor of renovating or restoring residential homes on Sunshine Street to small professional offices rather than seeing them demolished and the area turned into strip malls. This restoration of homes increases the value of property along Sunshine Street and allows commercial establishments to move into the area without ruining the residential aesthetic of the neighborhood.

Neighborhood Entrance Signage



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Neighborhood Entrance Signage

Sunshine Street community members voted in favor of having a small scale neighborhood entrance sign. They chose this style because they felt it reflected the character of their neighborhood the best and was visible at both a pedestrian and a vehicular scale.

Commercial Signage

Sunshine Street community members voted in favor of small scale free standing signage as opposed to the large scale, highly commercialized style. They stressed that height and material qualities should be specified so the signage will remain at a human scale and will not over power the residential aesthetic of the neighborhood. In addition, the signage that would be attached to commercial buildings should have an artsy character to it and no neon lit signs would be tolerated. Neon signs are associated with highly commercialized areas and they would be distracting at night to drivers and residents of the area.

Commercial Signage

Free Standing Signage



9



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Attached to Building



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Visions

- Vision 1 by Christine Friederich
- Vision 2 by Carly Rickerson
- Vision 3 by Kelsey Stein
- Vision 4 by Lannette Guerra
- Vision 5 by Audrey McNamara

Sunshine Street Corridor



Visions

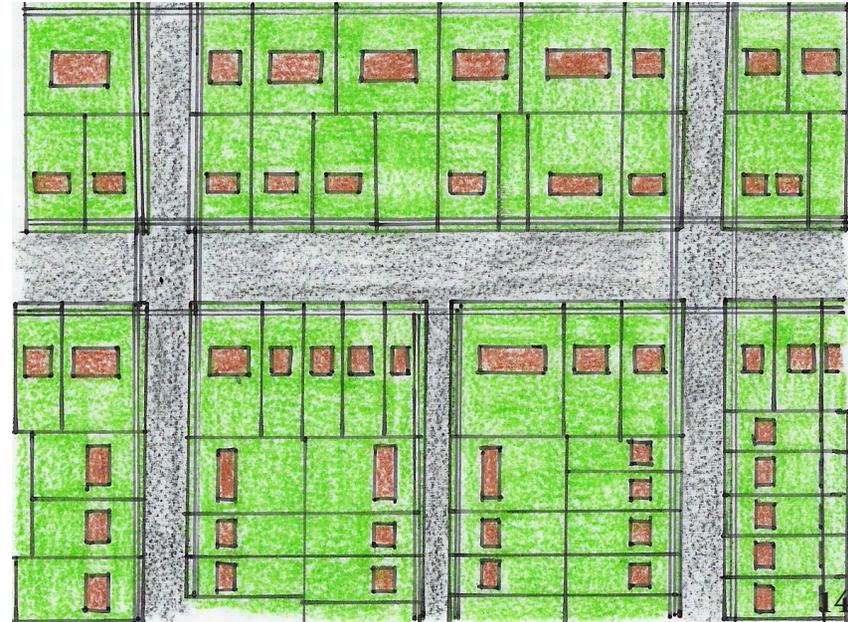
Vision 1

Christine Friederich

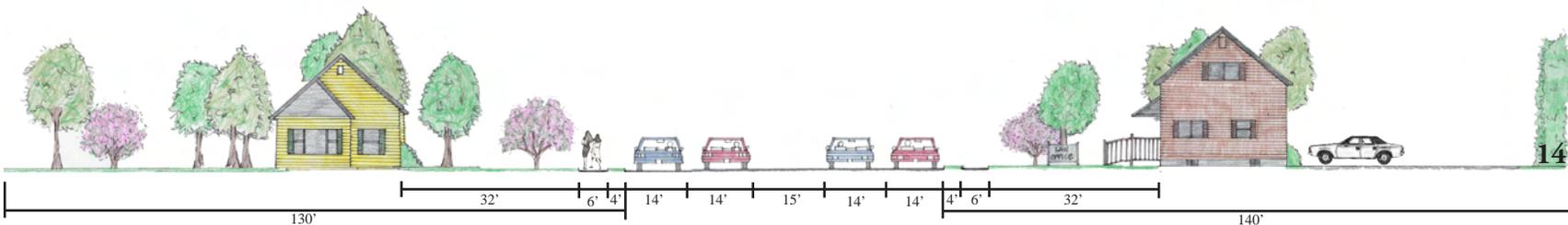


I interpreted the project challenge to inform and prepare the residents of the Sunshine Street neighborhood of the potential growth of commercial development in the future. By developing a vision that would allow commercial development in a residential style it would protect the existing neighborhood identity, fulfill the needs and wants of the residents, and allow the neighborhood to become economically viable.

My intentions for the concept of Vision 1 is to beautify the existing conditions of the Sunshine Street corridor and allow 90% of the street to remain single family detached homes and the remaining 10% be renovated homes converted into professional offices. Although this may appear as the ideal solution to current residents, it is the most susceptible to conversion to the unwanted commercial development that occurs on either side of this corridor.



Vision 1 Site Plan of the Springfield Visioning Corridor



Written Description

The proposal for Vision 1 is minimally invasive. It allows the Sunshine Street corridor to remain 100% residential in style, allowing 90% of that to be single family detached homes and 10% to be existing homes renovated into professional offices. These professional offices would have a very low amount of traffic and parking would occur behind the office and be accessed by a side street or alleyway. The parking area would have a pervious surface so storm water runoff would not become a problem, and there would be a vegetated buffer between the parking lot and the properties it backs up to. The rental properties that will not be changed into professional offices will have stricter rules about the upkeep of their premises. In addition, all the trees, plants, and shrubs that were lost in the ice storm this past January will be replanted throughout the neighborhood.

To make the area more pedestrian friendly, this vision calls for a four foot strip of grass to be placed in between the sidewalk and the street to provide a barrier between the pedestrian and the vehicle. Also the sidewalks will be widened to six feet allowing two to three people to walk side by side, or two groups of people to pass one another without having to step into someone's yard or out into the street. In addition to the sidewalk changes, crosswalks will be painted on the road at every major intersection to alert the driver of a pedestrian crossing.

Finally, existing open spaces, such as open lots, will be converted into community gathering spaces. These spaces will allow neighbors to gather, sit, and socialize. Also these spaces have the potential to be used for holiday events, a farmer's market, and even a community vegetable or flower garden. The upkeep of this area could be dependent on the city, the parks board, or the community itself.

Currently existing on Sunshine Street

Proposed renovations and improvements

Converting rental properties to professional offices



Placing a 4 ft green strip in between the pedestrian and the vehicle



Converting open lots to community green space



Visions

Vision 1

Christine Friederich

Strengths of Vision 1

Converting Properties to Professional Offices

Throughout the last few years, homeowners who live in the Sunshine Street area have been complaining about the poor upkeep of the rental properties along Sunshine Street. The rate of homes turning into rental properties is rapidly increasing, therefore the image of the neighborhood and property value is decreasing. By allowing small professional offices such as a law office, dentist office, or small art studio to move in and renovate the properties, the upkeep of the properties would improve tremendously.



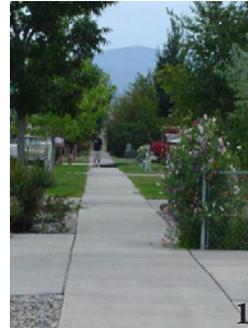
Convert Open Lots to Community Green Spaces

Introducing green spaces into the area is a great way to infill the open lots, create a community gathering space, and increase the property value along Sunshine Street. The area can be vegetated with many trees and flowers, and provide a place to sit and talk with neighbors. This area could house a community garden where neighbors would work along side one another thus creating a better sense of community throughout the neighborhood.



Improvements to the Sidewalks

The neighborhood will become more pedestrian friendly if the sidewalks are widened and there is a barrier placed between the pedestrians and the vehicular traffic. According to the Ozark Transportation Organization, a primary arterial, such as Sunshine Street, is required to have six foot wide sidewalks so two to three people can walk side by side, or two groups of people may pass each other without having to step off the sidewalk. In addition, a minimum barrier of a four foot strip of grass is to be placed in between the sidewalk and the road to protect pedestrians from vehicles. This four foot strip could be vegetated with plantings or trees that would improve the barrier for the pedestrians and give the neighborhood a recognizable identity. The examples to the right display these characteristics.



Provide Crosswalks for Pedestrians

Crosswalks will be painted on the roadway to alert drivers of a pedestrian crossing. These crosswalks will be placed at every major intersection to make it more convenient for pedestrians to cross the street. The crosswalks are especially needed for the children who live in the neighborhoods adjacent to Sunshine Street and attend Sunshine Elementary School.



Visions

Vision 1

Christine Friederich

Weaknesses of Vision 1

Encroachment of Commercial Development

Issues and Concerns:

Although this vision may sound ideal for the residents, it is by far the most susceptible to change to the commercial development that is on either side of it within the next twenty years. The upkeep of the rental properties has plummeted during the last few years. If stricter regulations are placed upon them, it would be easier for owners to sell them to land developers than to provide the upkeep needed. If land developers then buy them up and build strip malls or chain restaurants in their place, then the property value of the few owned properties on either side and behind the commercial development decline immensely.

Recommendations:

Zoning codes should be implemented that limit what can and cannot be built in the neighborhood. These codes would limit the scale, materiality, and use of facility. It is recommended that the buildings stay at a two or three-story limit, and they are designed to fit with the existing residential architectural style. By allowing small businesses and professional offices to move in, the image of the neighborhood will improve immensely. This allows the neighborhood to remain 100% residential style, prevents and protects property owners from the unwanted commercial encroachment, and increases the property value along Sunshine Street.



Parking for Commercial Development

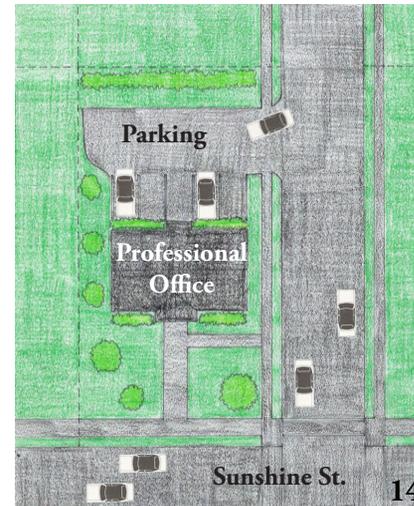
Issues and Concerns:

According to City of Springfield's existing zoning codes, no parking can occur on the street or in front of the house in this single family neighborhood. Therefore, parking must occur behind the facility and be accessed by a side street or an alleyway. When the idea of commercial development was presented to the community, they were very concerned about whether there would be enough parking and worried about it being invasive on their property. Since this vision proposes only the renovation of existing homes into professional offices, very little parking is needed. After measuring the lot sizes and researching the sizes needed for a parking lot, it was found that one row of parking with four to five parking spots would fit on the north side of the street, and two rows of parking with about eight to ten parking spots would fit on the south side of the street. (Refer to the diagrams on the right to see the layout of each style of parking.) Although these parking lots fit in the allowed space, community members were very skeptical of how they would influence the properties beside and behind them.

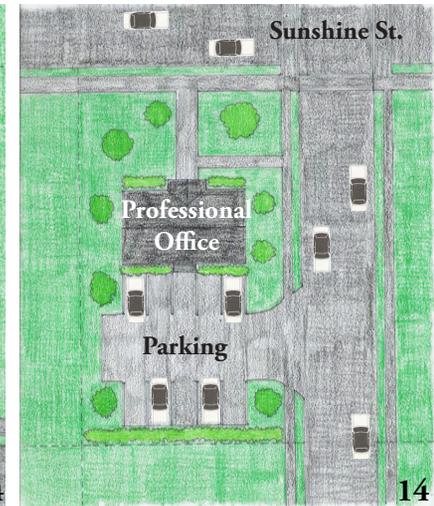
Recommendations:

Zoning codes should be implemented stating the size and materiality of the parking lot and the minimal size of buffers. It is recommended that the parking lots be made of a pervious surface so storm water runoff does not become a problem. Also a minimal eight foot privacy fence or vegetated buffer must be placed in between the parking facility and the homes to protect the privacy of the residents. The commercial facility will be responsible for the maintenance of the pervious surface of the parking lot and the vegetated buffer.

North Side of Sunshine Street



South Side of Sunshine Street



Examples of Pervious Surfaces



Examples of Buffers



Visions

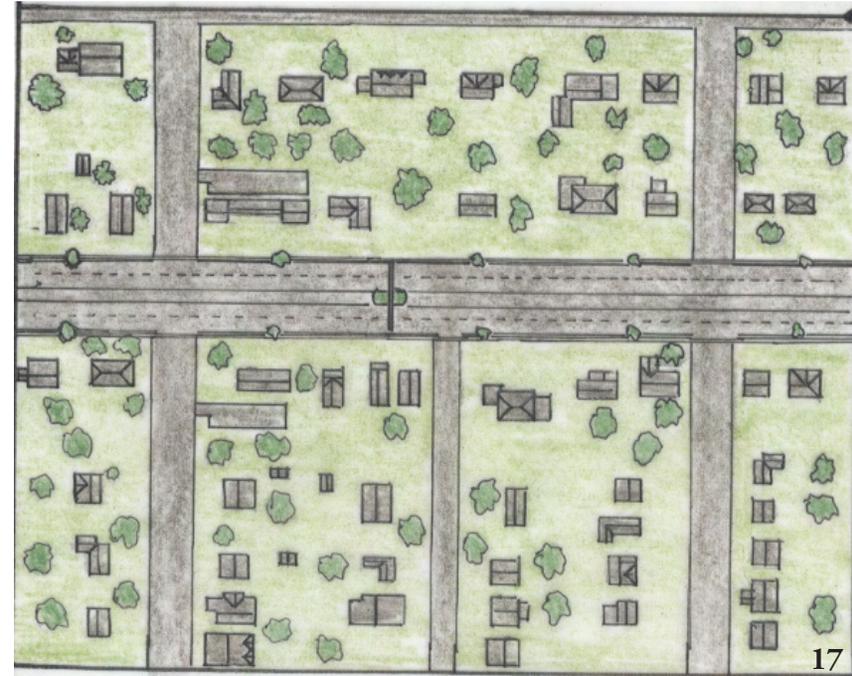
Vision 2

Carly Rickerson

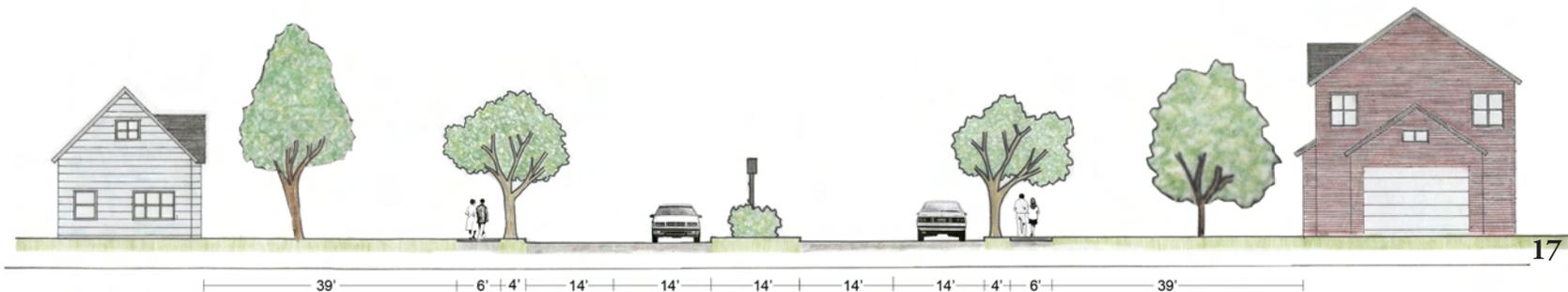


I view this project as an opportunity to revitalize and re-establish the community, which is in danger of being overtaken by commercial development. The biggest challenge of this project is finding a solution which will preserve and improve the quality of the neighborhood while respecting the street as a major element of the site.

My intentions for Vision 2 is to preserve the residential quality of the neighborhood by allowing limited commercial use, such as small businesses and office spaces. Like in Vision 1, existing homes are converted to commercial use, however, 80% of the buildings remain single family residential homes.



Vision 2 Site Plan of the Springfield Visioning Corridor



Written Description:

Because the commercialization of Sunshine Street has increased over the years and the majority of the residences on Sunshine Street are rental properties, it is likely that within the next few years this section of Sunshine Street will be completely commercialized. Our goal is to prevent large corporations and chain businesses from entering the community by implementing restrictions and limitations as to what types of businesses will be allowed in the future. It is highly unlikely that this community will remain 100% residential housing in the decades to come, so in order to protect the surrounding neighborhoods from the damage that large businesses can do, this vision proposes to limit any commercial activity to small businesses and professional office spaces, as well as small cafes, so long as the residential image of the community is maintained.

An important characteristic of a safe and sustainable neighborhood is one that caters to the needs of the pedestrian. Refuge islands with vegetation and textured crosswalks will allow the pedestrian to safely travel across only two lanes of traffic instead of five. This will promote greater contact between the north and south sides of Sunshine Street as well as keep the center turning lane for vehicles.

Establishing a recognizable character along this strip of Sunshine Street will add an attractive aesthetic to the community. It is a positive alternative to the businesses and signs which dominate the Sunshine Street corridor and will help to strengthen the residential image of the neighborhood. Replanting trees and vegetation will also replace many of the beautiful trees that were lost or damaged in the January ice storm.

Currently existing on Sunshine Street



Proposed renovations and improvements

Set limitations on what kinds of businesses are allowed



Pedestrian Crosswalks with Refuge Islands



Establish a recognizable character with trees and vegetation



Visions

Vision 2

Carly Rickerson

Strengths of Vision 2

Converting homes to businesses, offices and cafes

This proposal allows 80% of the neighborhood to remain residential, while 20% is converted to commercial. It includes converting existing homes for use as smaller businesses, offices and cafes rather than the larger businesses and corporations which dominate Sunshine Avenue beyond Kimbrough and National Avenues. The plans and interior spaces of the homes, as well as limited space for parking on the smaller lots, are better suited for small businesses with lower traffic volume.



Safer Conditions for Pedestrian Travel

Safety is an essential characteristic of a residential neighborhood and a major concern on Sunshine Street is automobile accidents. There are currently few crosswalks for pedestrians to travel from the north to south side of Sunshine Street. Pedestrians have about 20 seconds to cross five lanes of traffic while watching out for oncoming vehicles. Painted and textured crosswalks will help alert vehicles to crossing pedestrians and serve as a traffic calming device to slow down oncoming automobiles.



Planting Trees Along Streetscape & Green Spaces

An effective way to revitalize the neighborhood and create an attractive streetscape is to plant trees or other types of vegetation. Planting trees alongside the street has also been shown to reduce traffic speeds since the driver feels more enclosed and surrounded. Sprucing up the existing empty lots and green spaces in the area will create comfortable and inviting gathering spaces for the community, and enrich ties between neighborhood residents.



Preserving and Improving the Residential Style

By converting the existing homes into commercial use and limiting the amount of businesses into the neighborhood, the residential character of the neighborhood can be maintained. The limited amount of new construction allows this vision to preserve the beautiful homes in the area and protect the sense of community which exists in the neighborhood.



Visions

Vision 2

Carly Rickerson

Weaknesses of Vision 2

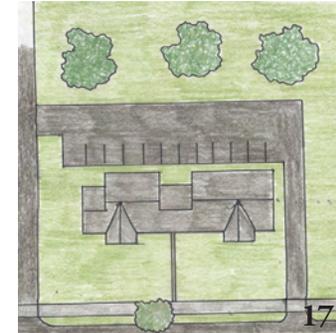
Joining Houses for Commercial Use

Issues and Concerns:

Though some of the adjacent homes on Sunshine Street are of similar style and size, the structure and interior spaces may not render themselves to be combined. Although businesses would benefit from the additional parking provided by combining the lots, combining smaller adjacent homes to create one larger building for commercial use may be problematic. The effort required to join these homes by making the adequate space requirements, not to mention different structure and materials, is discouraged.

Recommendations:

An easier and more practical solution is for businesses to convert larger homes for commercial use. There are several larger homes on the north side that would make adequate office or small business spaces. An alternative to this would be to demolish existing homes and rebuild a larger building for the business. This would also give the business the freedom to personalize the building according to their needs. If this were to happen, zoning codes and restrictions would need to be imposed to ensure that the residential quality of the neighborhood is not negatively affected and that all new construction is built in a residential style that compliments the community.



Bike Lane on University

Issues and Concerns:

Creating a bike lane on University Street has its advantages in that it would provide a safer route for bicyclists than on Sunshine Street, and promote the use of bicycle transportation. There is currently no bicycle route on Sunshine Street and dangerous traffic conditions prevent the residents of the area and those who pass through from riding their bike. University Street is quieter and has less traffic than Sunshine Street making it an ideal place for a bike lane. However, the residents on University Street frequently park their vehicles on the street, therefore placing a bike lane on University Street would prohibit the residents from parking on the street.

Recommendations:

The most practical solution is not to declare University Street as a bike route, but to simply recommend that bikers use this street rather than Sunshine Street when riding their bike. This would allow the residents to continue to use the street for parking. Due to the low traffic volumes and speeds on University Street, it is not necessary to designate a lane for bicyclists. Riding on the street with the vehicles is a much safer alternative to riding on Sunshine Street.



Visions

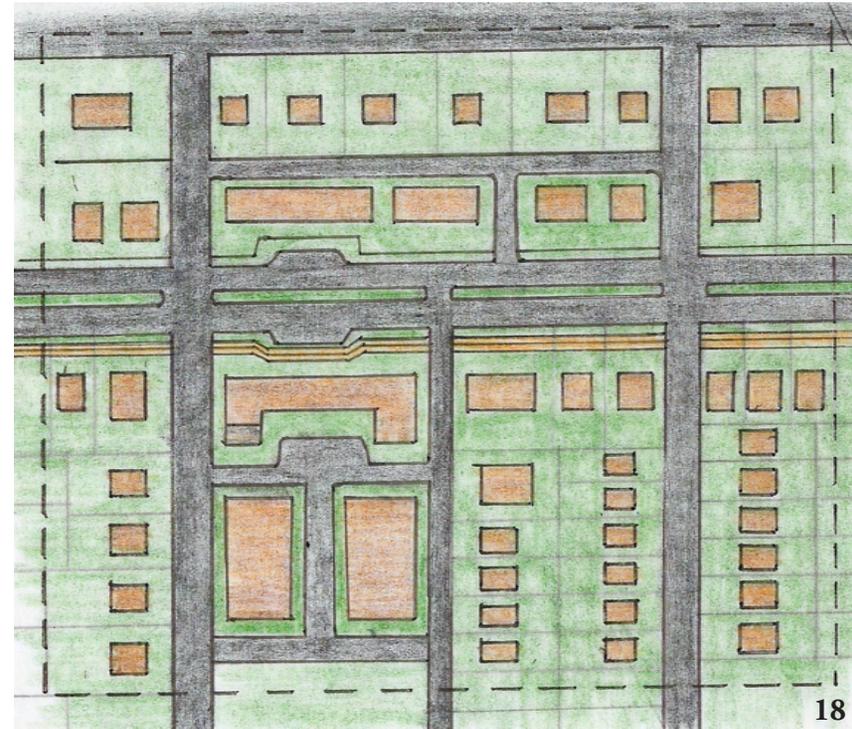
Vision 3

Kelsey Stein



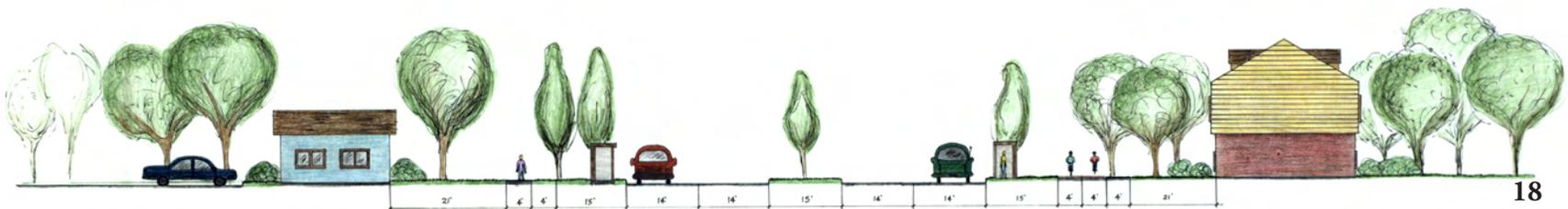
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The project challenge was to maximize the economic viability of the area (increasing property value) while still maintaining a residential character. Vision 3 increases property value while maintaining this residential character by incorporating small businesses, mixed-use developments, and high-density housing. The property value will also be increased by beautifying the streetscape. This would be done through a number of improvements to crosswalks, sidewalks, lighting, the median, and bus and bike access.



18

Vision 3 Site Plan of the Springfield Visioning Corridor



18

Written Description:

Vision 3 contains 50% existing residential and 50% commercial/mixed-use development. These new developments would be in a residential style. High-density housing in the form of townhouses and condominiums would increase property value. High-density would be placed on the north or the south side of the street. Professional offices could also be placed in existing converted homes on the north side of the street because lots are narrow and cannot handle a large influx of vehicles. Mixed-use would also increase the property value of these lots. These would be placed on the south side of the street only where the lots are deeper and can handle higher traffic.

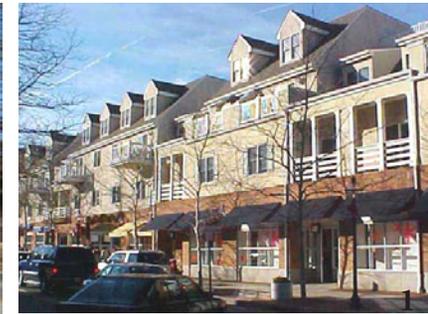
The development of high-density and mixed-use facilities would decrease curb cuts allowing a vegetated median. This vegetated median would decrease traffic speeds and add to the quality of the pedestrian and vehicular experience on Sunshine Street. Bus turnouts would be provided in front of commercial areas and green spaces so they would not disrupt the single family homes access to the mixed-use areas would also be easier.

Streetscape improvements would increase the property value of the Sunshine-Holland and University Heights neighborhoods. Textured crosswalks, wider sidewalks, decorative lighting with banners, and vegetation would improve the streetscape. Bike lanes and other forms of alternative transportation would ease traffic congestion along Sunshine Street.

Currently existing on Sunshine Street



Proposed renovations and improvements



Visions

Vision 3

Kelsey Stein

Strengths of Vision 3

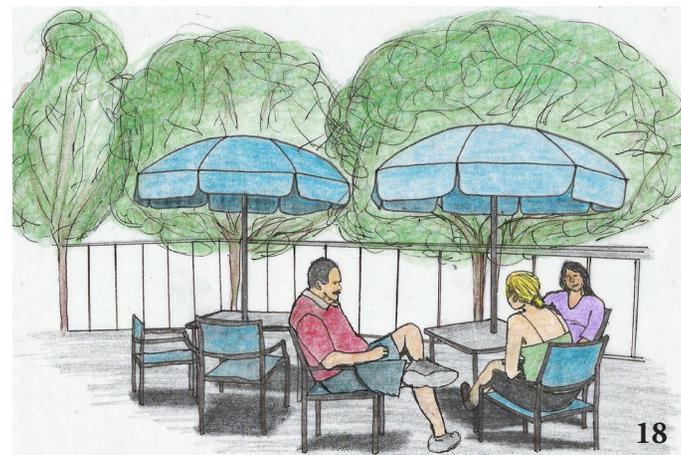
Mixed-Use

The strengths of this proposal are the incorporation of mixed-use and high-density housing. On the south side of the street lots are deeper making access from side streets easier and allowing for more parking in these higher traffic areas.

Mixed-use would increase property value by providing commercial activity and maintain the residential character of the neighborhood by being half residential. The style of the building would reinforce this residential character and could be established through ordinances or codes.

These codes could also specify which business would be allowed to operate. This would also maintain the residential character of the neighborhood. For example, businesses that would be allowed to operate include, but are not limited to: professional offices, small shops/boutiques, and small cafes/bakeries.

Some of these areas could have outdoor seating and/or outdoor plazas that could also be designed into these mixed-use facilities. This would foster a greater sense of community within the Sunshine-Holland area.



Vegetated Median

The vegetated median is a recommended design guideline established by the Ozark Transportation Organization for a primary arterial (the classification of Sunshine Street). A vegetated median has been proven to slow traffic. The corner of National Avenue and Sunshine Street is rated F for dangerous intersection, the worst rating available. This proves traffic calming devices, such as vegetated medians are necessary along Sunshine Street. A vegetated median, if properly landscaped and maintained, would also increase the property value of the entire street and could give the area a sense of identity.



Streetscape Improvements

A vegetative barrier between the pedestrian and vehicular zones is also a design guideline established by the Ozark Transportation Organization for a primary arterial. A vegetated barrier protects the pedestrian physically and makes them feel more safe. Vegetative barriers are also a traffic calming device, helping to decrease traffic speeds. The combination of a vegetated median and vegetative barrier could create a tree canopy over Sunshine Street and further add to the sense of identity. A vegetated barrier, if properly maintained, could also increase the property value of the street. Textured crosswalks, widened sidewalks, and lights are also assets that improve the streetscape and increase property value.



Visions

Vision 3

Kelsey Stein

Weaknesses of Vision 3

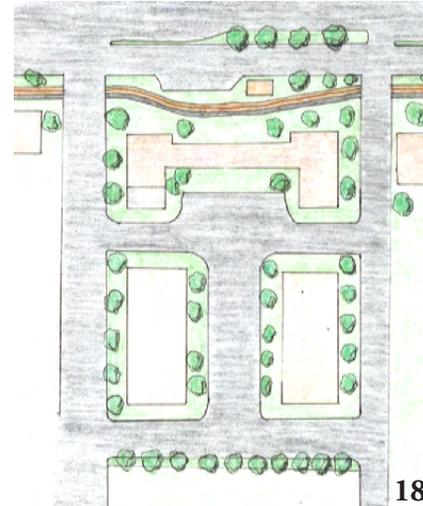
Invasiveness of Vision

Issues and Concerns:

The major issue regarding Vision 3 is the invasiveness of the project. On the south side of the street the mixed-use and high-density developments would extend further into the Sunshine Holland neighborhood than just the lot bordering Sunshine Street. This may be opposed by some community members. The height and scale of the mixed-use buildings may also be opposed. High-density may also be opposed because of the misconception that it will bring low-income residents to the neighborhood and decrease property value.

Recommendations:

To maintain a residential character in a mixed-use building guidelines must be established and followed. These guidelines will pertain to the scale, materials, amount of vegetation, and type of business that will be allowed in the mixed-used area. By maintaining a residential character, the mixed-use building will blend into the existing landscape. Guidelines will also have to be established for high density housing. Only townhouses will be allowed and these must also be in a residential style.



Vegetated Median and Bike Lanes

Issues and Concerns:

The vegetated median will be opposed by some members of the community living on Sunshine Street. It is already very difficult to turn left on Sunshine Street due to the high traffic speeds and volumes. A vegetated median could increase this difficulty. The two-way bike lane is also a weakness because, according to the Springfield Transportation Department, cyclists should be treated as vehicles and not pedestrians. Separating cyclists from vehicles increases the likelihood of an accident.

Recommendations:

The design guidelines specified by the Ozark Transportation Organization recommend no curb cuts along a primary arterial. Currently, Sunshine Street has an abundance of curb cuts due to the large number of single family homes along the street. The vegetated median should only be installed once a large portion of properties have been converted to mixed-use or high density housing. Access into these areas from side streets also decreases the number of curb cuts necessary. When the number of curb cuts along Sunshine Street has decreased significantly a vegetated median will be met with less opposition because of the decreased amount of single family homes fronting Sunshine Street.



Visions

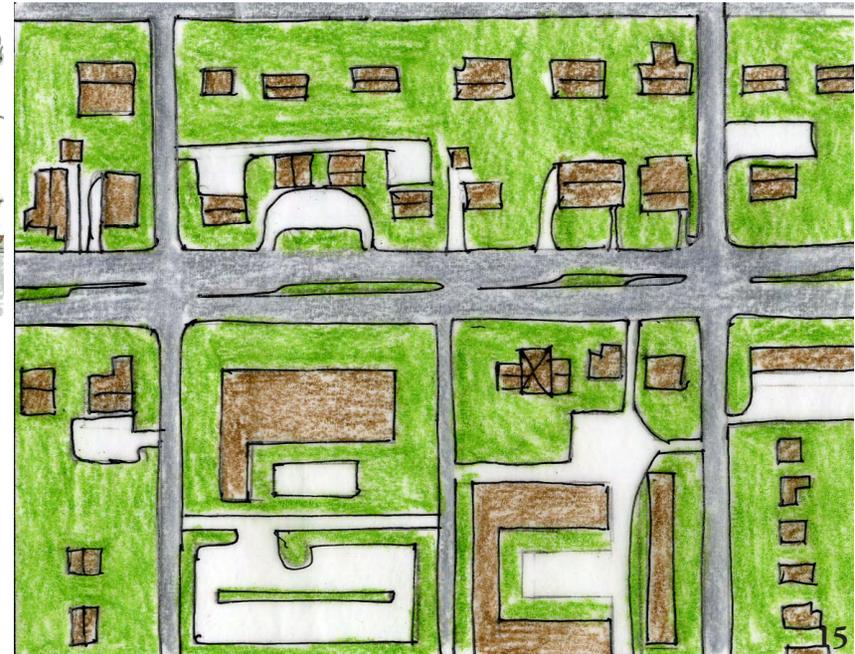
Vision 4

Lannette Guerra

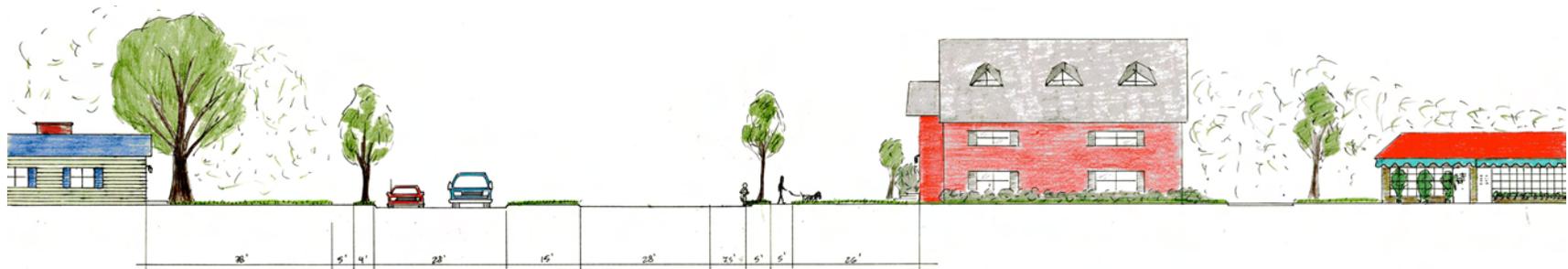


Vision four introduces 60% of mixed use and high-density while 40% remains residential single family homes. The challenge is to integrate these aspects into the neighborhood while maintaining the residential character.

The final vision for Sunshine Street should become a model for other neighborhoods experiencing commercial encroachment. The design of Sunshine Street should support the idea of a shared space between residential, office, and retail. Most importantly the design shall always maintain the scale and appropriate materials common to the surrounding neighborhoods. These aspects will help preserve the positive aspects of the area while adding economical viability.



Vision 4 Site Plan of the Springfield Visioning Corridor



Written Description:

The proposal for Vision 4 incorporates a higher percentage of the features introduced in the prior three visions. This vision allows for 60% mixed-used and high density, 20% professional offices, 15% single family homes and 5% green spaces. In order to better integrate mixed-use and high-density into the neighborhood, a series of street renovations would have to occur. The streets would be lined with dogwood trees between the road and sidewalk, which not only provides a vegetated barrier between cars and pedestrian, but also beautifies the Sunshine Street corridor. The surface treatment and vegetated buffers along the sidewalk act as visual indicators to help strengthen the identity of neighborhood and add to the positive aesthetics of the area. The incorporation of a grassy median that will include decorative lighting and pedestrian crosswalks, will also help to enhance safety as it acts as a psychological signal for vehicular traffic to slow down.

This vision calls for the development of mixed-use and high-density housing. The majority of the high-density homes will occur on the south side of the street because the lots are deeper and allow for more parking. The integration of high quality residential housing will help maintain the residential character of the neighborhood, while providing a positive alternative to the typical apartment complex and strip center seen elsewhere on Sunshine Street. The buildings facing Sunshine Street would be limited to two stories and should maintain the current material palette of the neighborhoods.

The conversion of residences into commercial and/or the construction of new commercial facilities would occur throughout the length of the Sunshine Street corridor. These shops would provide basic amenities for residents such as small boutiques and professional offices within a short walking distance.

Currently existing on Sunshine Street



Proposed renovations and improvements

Converting existing apartments into Town homes



Placing a vegetated barrier between pedestrian and roadway



Incorporating a grass median with lighting



Visions

Vision 4

Lannette Guerra

Strengths of Vision 4

Union between North and South

The introduction of vegetation, gateways, and building of similar styles along both sides of the Sunshine Street corridor can visually strengthen the appearance and unity of the area. It creates an overall sense of identity.



Improvements to Sidewalks and Bike Lanes

Although bicyclists do utilize Sunshine Street there is no designated area for them. This dangerous condition can be improved with the addition of bike lanes. Another improvement would be the addition of sidewalk pavers. The pavers could help to better distinguish the different zones between bicyclist, vehicular, and the pedestrian realms.



Converting Aging Homes into Townhouses and Mixed Use

Adding mixed-used and high-density should not be as worrisome as many make it out to be, it can be as simple as restoring an old home to its glory years and bringing in a small flower shop to cater to you when you have to run for that last minute gift idea.

Townhouses are also a great alternative to apartments, because they are easily maintained and promote for residence of a long term stay plan and they don't necessarily have to all face the main artery. Overall mixed-use would only exist to cater to the residence living in the townhouse and adjacent neighborhoods, but respecting the ordinances giving to the area such as; no Wendy's, no noisy guitar shop, no shop with repugnant odors and flashy neon decor.



Safety Achieved Through Traffic Calming

According to the Ozark Transportation Organization a traffic calming device ranges from medians on a major arterial to bus stops every 30 minutes. In Vision 4, a number of traffic calming devices are inserted to help with speeding and number of accidents recorded each month on the intersection of National Avenue and Sunshine Street. The first major help mechanism is a grassy median. Even though we are not subtracting any footage from the traffic lanes, the median gives the illusion of less space causing the cars to slow down. A row of consecutive trees on both sides of the street encroaches upon the driver and causes them to reduce speed.



Visions

Vision 4

Lannette Guerra

Weaknesses of Vision 4

Maintenance of the Sunshine Corridor

Issues and Concerns:

Because this vision incorporates a higher percentage of vegetation, textured sidewalks and recreational areas it requires regular upkeep. If the corridor is unkept, its identity and positive aesthetics are lost. The major concern with the above statement is that, “Sure we all agree with keeping our neighborhood well groomed, but who is going to pay for the maintenance on the corridor? Are these changes going to significantly raise our taxes?”

Recommendations:

The Sunshine Street Corridor, although considered a primary arterial, is currently managed by the City of Springfield, Missouri. Therefore, the modifications and the maintenance of the Sunshine Street corridor is the responsibility of the City of Springfield.



Fear of commercial development in the area

Issues and Concerns:

Because Vision 4 introduces a higher percentage of mix-use, high density and professional offices, the fear of unwanted commercial businesses spreading throughout the neighborhood has surfaced amongst the community. “How do we really stop Wendy’s from taking over the 500 block?” “How can I make sure that when I wake up in the morning I don’t smell a restaurant’s prep work?” “How do I truly know I won’t be affected by a photo studio’s dark room chemicals?” These are all valid concerns raised by the residents north and south of Sunshine Street.

Recommendations:

Zoning Codes and City Ordinances can control the structure and sensory elements of a neighborhood. We recommend that the city and neighborhood residents collaborate and develop planning and design guidelines specifically for this section of the Sunshine Street Corridor. These guidelines for the Sunshine Street Corridor overlay district should cover land use, parking, appearance, scale and signage.

Current Condition between Jefferson and Campbell



Verses a strictly controlled area



Visions

Vision 5

Audrey McNamara



16

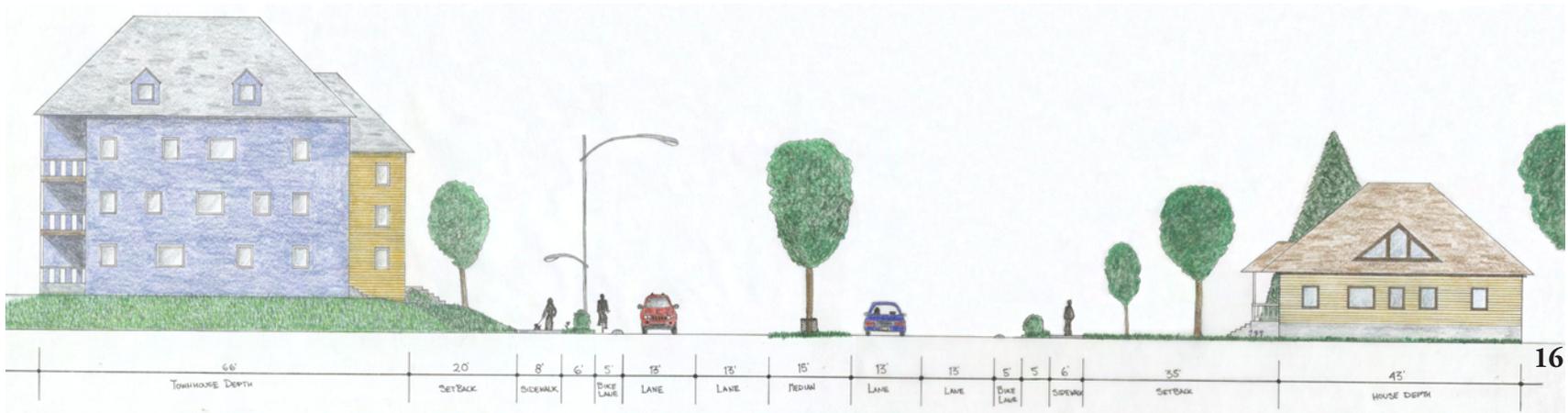
I interpreted the project challenge as trying to create a vision that would provide a more economically viable area while retaining the “residential character” of the Sunshine Street corridor.

My concept was to create staggered pockets of varying densities of mixed-use within the single family neighborhood along Sunshine Street. Special attention was paid to the creation of a mixed-use district that would not be over bearing to the pedestrian or the vehicular traffic.



16

Vision 5 Site Plan of the Springfield Visioning Corridor



16

Vision 5 Description:

Vision 5 is the most developed enhancement of the Sunshine Street corridor. It is meant to be a vision of what could happen thirty or more years down the road. Vision 5 proposes an approximate land use of 30% existing residential single-family dwellings and 70% mixed use renovated and new construction. High-density and transitional, extended stay townhomes are proposed. A new extended stay development to the south of Sunshine Street near St. John's Hospital should be developed for the families of patients to reside in while being in close proximity to the hospital.

Traffic calming elements can be found throughout Vision 5 such as the construction of a fifteen foot wide vegetated median with turning lanes, the addition of bike lanes to either side of Sunshine Street, bulb-outs on street corners with textured or painted crosswalks and refuge islands at crosswalks along with textured paving to designate the crosswalks. Bus turnouts would be located in front of green spaces and businesses along Sunshine Street so as not to encroach upon the front yards of the residential. In order to facilitate a more pedestrian friendly area along Sunshine Street, pedestrian scale lighting and textured sidewalks would also be an added feature.

Vision 5 also significantly alters the density of the build environment and with it the variety of uses available. This enables a more economically viable and socially interactive neighborhood. The increase in density allows more people to live as well as work in the neighborhood. Commercial uses encourage social interaction by bringing people from the residential neighborhood together and providing goods and services that they use.

Currently existing on Sunshine Street

Proposed renovations and improvements

Multi-family housing



Bulb-outs at corners with cross walks



Developing various uses and densities



Visions

Vision 5

Audrey McNamara

Strengths of Vision 5

Integration of commercial and residential uses

One strength of Vision 5 would be the integration of remodeled and newly constructed buildings containing mixed uses into the existing residential neighborhood fabric without losing the character of the area.



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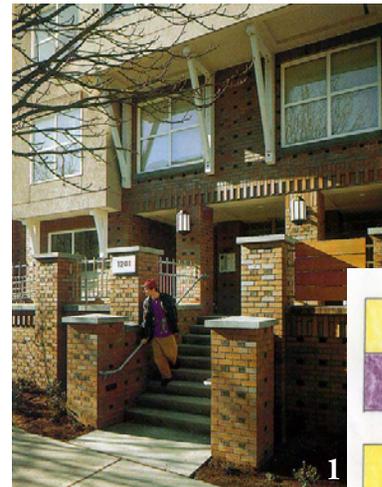
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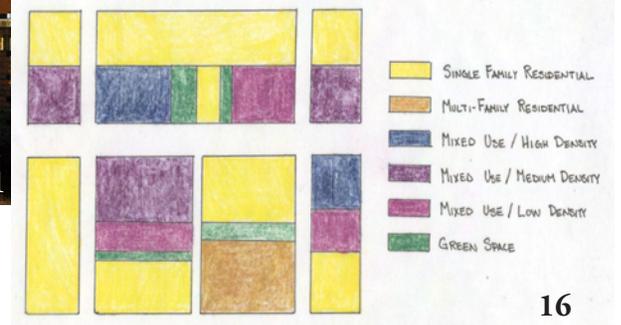
Varying the densities of new and existing buildings

Another strength of Vision 5 would be the strategic placement of the various degrees of density. By spreading out low, medium, and high-density structures the corridor will demonstrate the gradual change from single family residences to mixed-use high-density construction over a significant amount of time. Green space would also be used as a buffer between the existing residential homes and the much more dense mixed-use development.



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Hypothetical Block Density Diagram



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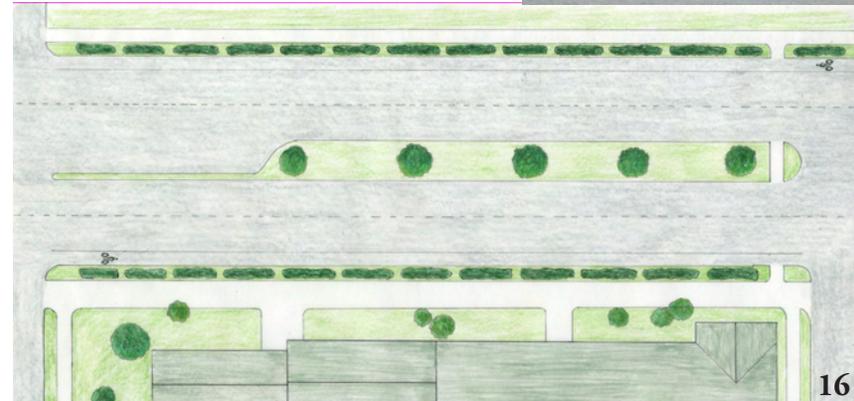
Pedestrian friendly streetscape

Creating pedestrian friendly streetscapes that provide protection from the vehicular traffic is also a strength of Vision 5. This includes wider sidewalks anywhere from six feet all the way up to the building storefronts. Textured sidewalks that widen and repair the existing concrete sidewalks would be added, along with vegetation between the pedestrian and motor vehicles. Pedestrian scale lighting would also be added to encourage activity along the street as well as increase the safety in the neighborhood.



Vegetated median with turning lanes

A vegetated median would define the residential neighborhood for the vehicular traffic driving along Sunshine Street specifically delineating it as something very different from the typical strip mall commercial developments to the east and west. This is made possible by the significant increase in mixed-use as opposed to the single family residential, which allows for many of the curb cuts to be eliminated. The addition of turning lanes in the median will allow for access to all the intersecting side streets. In addition the median creates an area of refuge for the pedestrians using the crosswalk on Sunshine Street.



Eventually 100% Commercial

Issues and Concerns:

Implementation concerns centered around the ability that once 70% of the area has been converted from single family detached residences to mixed-use, what would prevent the area from going 100% commercial. There is concern that even if changes are made that the area will eventually become like the rest of Sunshine Street and thus reduce the property values of the surrounding residences in the adjacent neighborhoods.



Recommendations:

However, if mixed-use structures begin to dominate the streetscape along Sunshine Street, they will create new housing opportunities. As a mixed-use building is the desired typology it will, in many cases, contain residential units. So, even if there are no single family residences exist there would still be people living in the buildings on Sunshine Street.



In conclusion, there will need to be many gradual changes that enhance aspects of the area for many years before the Sunshine Street corridor could evolve into this proposed vision.



Appendix

- Zoning
- Funding Sources
- Community Involvement and Resources

Sunshine Street Corridor



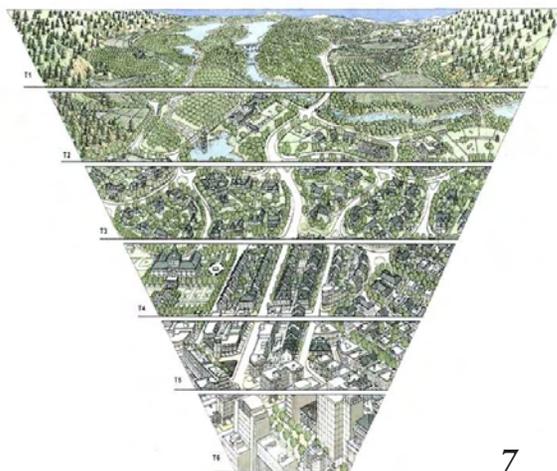
Appendix

Zoning Recommendations

There are many alternative resolutions to the existing zoning issues and concerns. In light of this, the City of Springfield has suggested a preferred method of addressing this particular zoning situation. This would be to create an overlay district using a form-based code as a visual guideline for future neighborhood enhancement.

The proposed overlay district would impose a variety of specific regulations and conditions for commercial and mixed use activities within the residential neighborhood. This overlay district would appear as an optional feature over the existing residential zoning to add flexibility rather than restriction to the underlying zone.

Transect Triangle Diagram



“A community’s physical form—namely, its buildings, streets, and public spaces—signifies its most defining characteristic (Dover, 2003) as they shape the public realm (Katz & Ferrell, 2003). Asserting more control over a community’s form could lead to improvements in the way the community functions (Ferrell & Madden, 2002). This increased control includes the fostering of pedestrian-friendly mixed-use developments, and a range of housing types.”

-- Jason T. Burdette, *Form-Based Codes: A Cure for the Cancer Called Euclidean Zoning?*

Clarifications of an “Overlay District” and “Form-based Code”

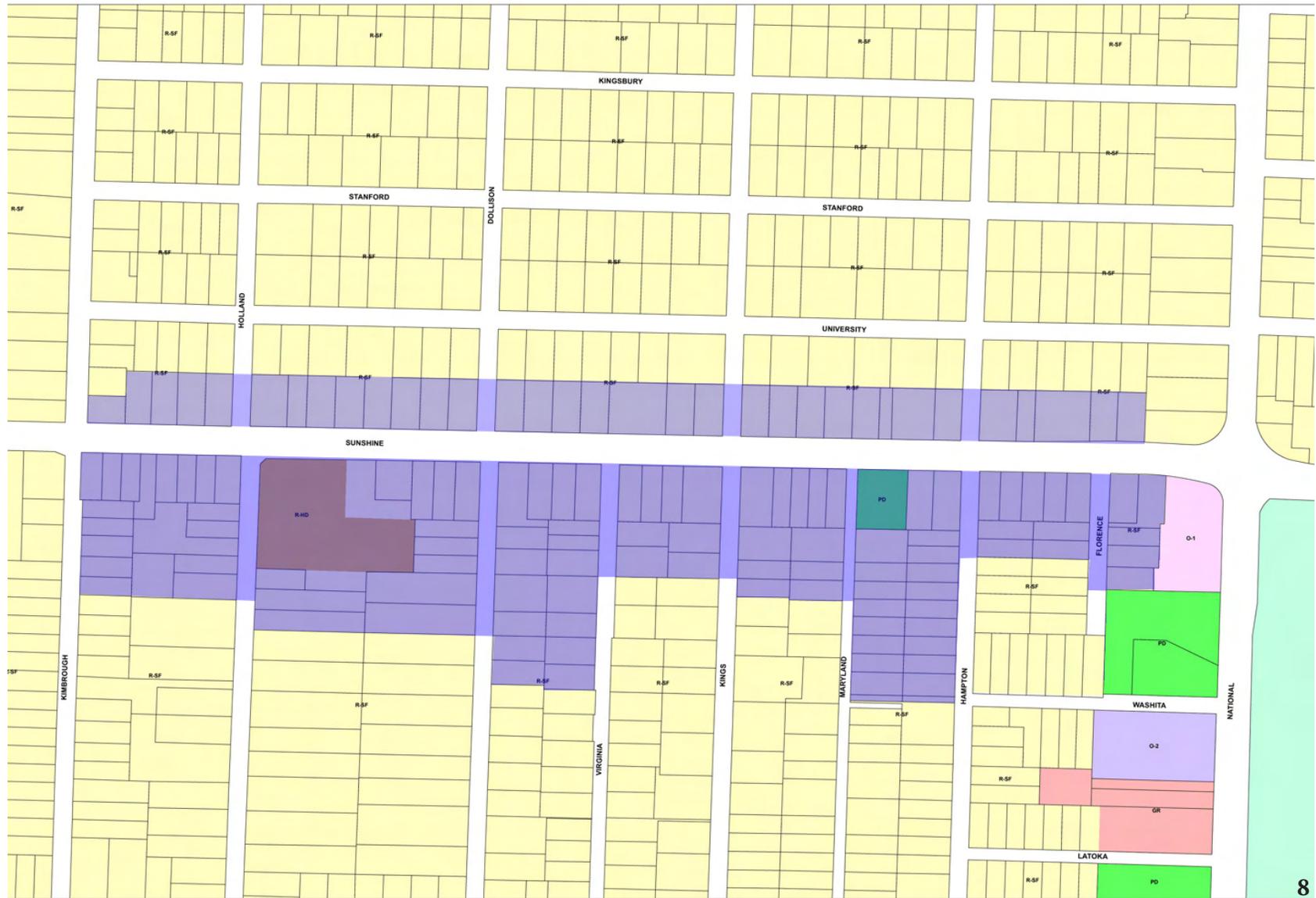
An overlay district is a zoning district over one or more existing districts that contains special features such as historic buildings, environmentally sensitive areas, downtown residential uses or other features. In this case the overlay district is meant to preserve the residential neighborhood character while sensitively incorporating a mix of uses in varying densities to create a more economically viable neighborhood that will maintain its character and prevent typical strip commercial development form occurring.

According to the Form-Based Codes Institute, form-based codes are “a method of regulating development to achieve a specific urban form. Form-based codes create a predictable public realm by controlling physical form primarily, and land uses secondarily, through city or county regulations.”

Form-based codes focus on the existing forms, in this case the existing single family residences and their relationship to each other, to other potential structures, to adjacent streets and to open spaces, instead of the current code based primarily on the type of land use.

Form-based codes typically include a regulating plan to illustrate in detail the relationship between the streetscape and the adjacent neighborhood. They also include building envelope standards to regulate the built form and specify the height and siting of elements.

PROPOSED SUNSHINE STREET OVERLAY DISTRICT



Appendix

Sunshine Street Mixed-Use Corridor Overlay District

The following regulations establish the standards and procedures of the Sunshine Street Mixed-Use Overlay District that apply to all lots within the District boundary and to all land uses, buildings and structures lying on those lots.

Permitted Uses:

- Existing Single-family detached residences
- Townhouses containing no more than four (4) dwelling units
- Residential uses within an existing residence or newly constructed structure in which commercial activity takes place; provided such uses are located above the first floor or behind non-residential uses so as to promote continuous non-residential uses on the first-floor level along all street frontages; live/work facilities for example
- Home occupation uses, as permitted by Section 5-1100 of the Zoning Ordinance
- Bed & Breakfast / Inns
- Offices, administrative, business, finance and professional
- Noncommercial, non-profit residential neighborhood facilities, including community centers, offices and property owners associations and maintenance facilities operated by a neighborhood or community organization or a property owners association in accordance with the provisions of Subsection 5-2700 of the Zoning Ordinance
- Public & private parks and playgrounds

Signage:

- Each building containing a commercial use shall be permitted one (1) detached sign and either one (1) projecting sign or one (1) wall sign.
- **Size:** A maximum of twenty (20) square feet of effective area shall be permitted for each attached sign. Detached signs may be two sided, each side not to exceed fifteen (15) square feet.
- **Height:** No detached sign shall exceed a height of six (6) feet as measured from street grade.
- **Sign Lighting:** No sign shall use a blinking, flashing, animated, or other illuminating device which changes in light intensity. No beacons, spotlights, neon or strobe lights shall be permitted.
- **Sign Types:** Signs shall only refer to a business, person, activity, goods, products, or service located on the premises where the sign is installed and maintained. Temporary signs are not permitted. Only on-premise signs are permitted in this Overlay District.

Open Space Requirements:

- **Minimum open space:** Not less than twenty (20) percent of the total lot area shall be devoted to open space including required yards and bufferyards. Open space shall not include areas covered by buildings, structures, parking, loading and other paved areas and internal streets. Open space shall contain living ground cover and other landscaping materials.
- **Maximum impervious surface:** The combined area occupied by all main and accessory buildings or structures, parking, loading and other paved areas and any other surfaces which reduce and prevent absorption of stormwater shall not exceed sixty (60) percent of the total area unless modified in accordance with Subsection 6-1215.



Conditional Uses:

The following conditional uses may be permitted, provided they meet the provisions of, and a Conditional Use permit is issued. These uses shall either be permitted within an existing residence with approved exterior alterations if necessary or within approved new construction.

- Multi-Family dwellings
- Extended stay housing for multi-family or single-family use
- Day Care centers
- Personal service establishments limited to barber shops, shoe repair, express or mailing offices and hearing aid and eye glass shops.
- Offices, medical and dental.
- Retail establishments for the following uses: books, antiques, bakeries, flowers, gifts, clothing, and shoes.

Bufferyard Requirements:

- Whenever any development in this Overlay District is located adjacent to the surrounding residential district, screening and a bufferyard shall be provided in accordance with Sections 6-1000 and 6-1200 along with additional community required specifications.
- Specifics and suggestions on paving materials can be found in the design recommendations.

Use Limitations:

- All activities and permitted uses except off-street parking and day care activities shall be conducted entirely within a completely enclosed building.
- No individual retail store, personal service establishment or other permitted use shall have a gross floor area greater than two-thousand five hundred (2,500) square feet.
- All uses shall operate in accordance with the noise standards in Section 6-1500.
- No use shall emit an odor that creates a nuisance as determined by Chapter 2A, Article X, Springfield City Code.
- Streets through adjacent residential areas shall not be used to provide principal access for truck traffic to any non-residential use in this district.

Bulk & Intensity of Use Restrictions:

Maximum structure height:

- Main building: Thirty-five (35) feet.
- Outbuildings: Fourteen (14) feet.
- All structures shall remain below a forty (40) degree bulk plane as measured from the boundary of the surrounding residential district.

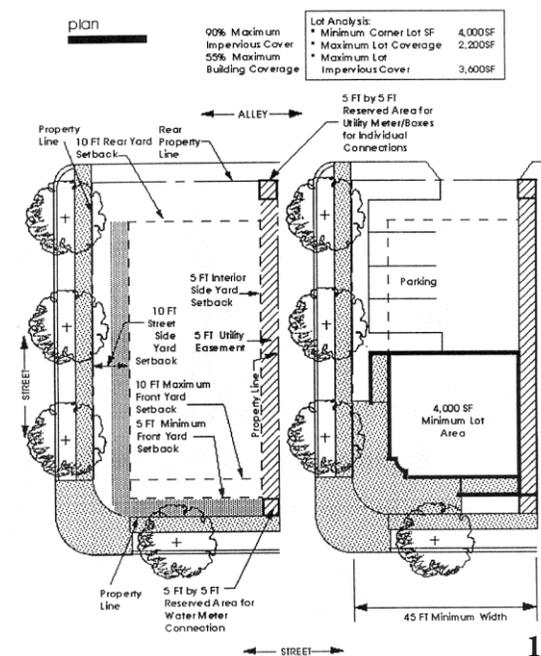
Minimum yard requirements:

- Front yard: Twenty-five (25) feet for residential uses, none required for commercial uses.
- Side yard: Five (5) feet.
- Rear yard: Twenty (20) feet.

Off-Street Parking Requirements:

- Non Residential uses shall normally have two (2) parking spaces for each 1,000 square feet of gross non-residential use.
- Parking surface should be as minimally invasive as possible allowing the maximum possible pervious area as well as maintaining the existing canopy trees in the area. Specifics and suggestions on paving materials can be found in the design recommendations.

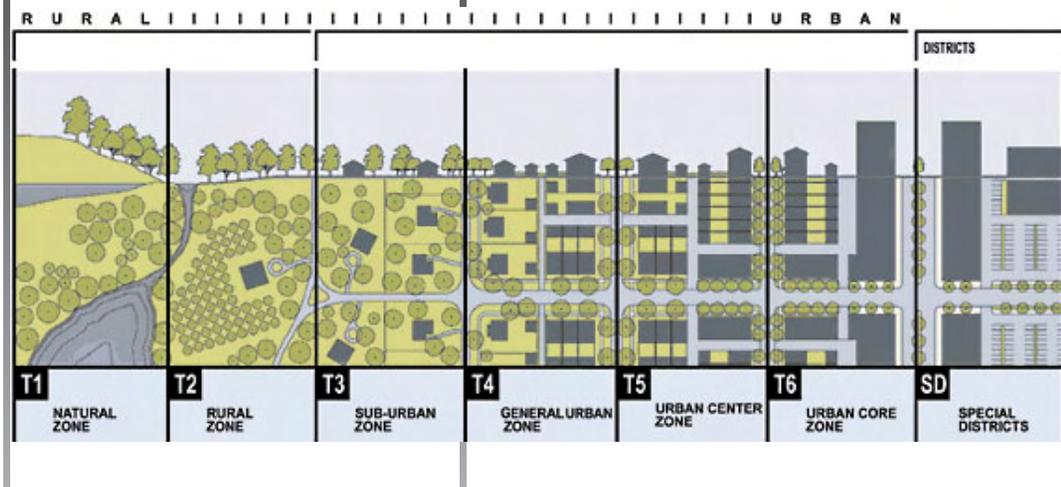
Typical residential and commercial lot layout



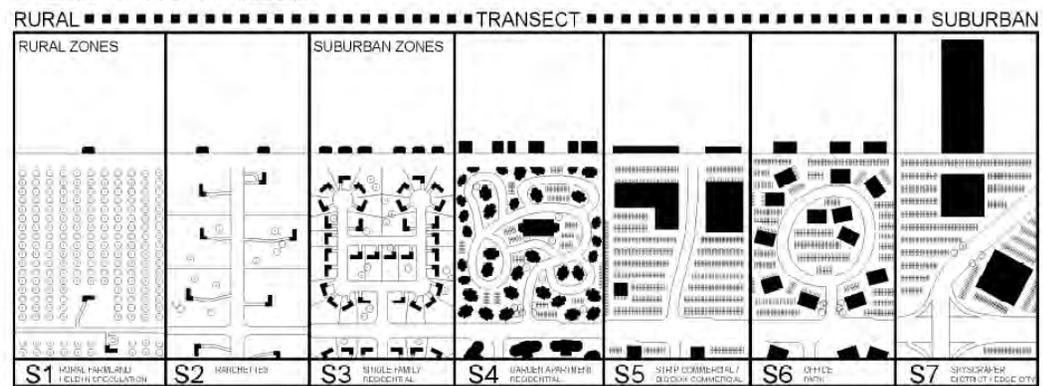
Appendix

Form-Based SmartCode of the Sunshine Street Mixed Use Corridor Overlay District

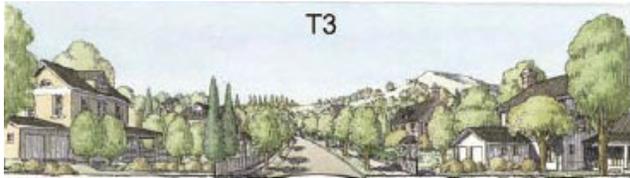
SmartCode is a modifiable format for the implementation of a form-based code. The SmartCode is broken down into zones that correspond to the transects devised by New Urbanists Andres Duany and Duany Plater-Zyberk & Company. A transect is a categorization system that organizes all elements of the urban environment on a scale from rural to urban. The Transect has six zones, moving from rural to urban. It begins with two that are entirely rural in character: Rural preserve or the Natural Zone (T1); and Rural reserve or the Rural Zone (T2) (areas of high environmental or scenic quality that are not currently preserved, but perhaps should be). The transition zone between countryside and town is called the Edge or Sub-Urban (T3), which encompasses the most rural part of the neighborhood, and the countryside just beyond. The Edge is primarily single family homes. Although this Sub-Urban area is the most purely residential zone, it can have some mixed-use, such as civic buildings like schools. Next is the General Urban Zone, (T4) the largest zone in most neighborhoods. General is primarily residential, but more urban in character (somewhat higher density with a mix of housing types and a slightly greater mix of uses allowed). At the urban end of the spectrum are two zones which are primarily mixed-use: Urban Center (T5) (a small neighborhood center or a larger town center serving more than one neighborhood); and Urban Core (T6) (a central business district serving the region). Typical sprawl development seen throughout the country is depicted in the Sprawl Transect which significantly contrasts the New Urbanist Transect in plan as well as in section.



THE SPRAWL TRANSECT



The area within the Sunshine Street Corridor should fall under the zone because of the street classification and its location within the larger context of the city of Springfield. The current density and land use however falls under the T3 zone. Because there is such a drastic change between the two zones, as can be seen in the illustrations below, and the objective is not to eliminate the existing single family residential, it would therefore be appropriate to classify the area as a Special District which would allow the existing aspects of the T3 zone while gradually incorporating aspects of the T4 zone. Special Districts can be customized to allow many different types of situations to occur.



7



7

SPECIAL DISTRICT (SD)

Special District designations are assigned to areas that, by their intrinsic function or size, cannot meet the requirements for any Transect Zone or combination of Zones specified in the SmartCode. The provisions of the existing local codes shall remain applicable to Special Districts. Special Districts should be created by the Planning Office in the process of preparing an Infill Community Plan. Alternatively, the conditions of development shall be determined in public hearing of the Legislative Body. The standards determined for this Special District are recorded in Table 15.

PRE-EXISTING CONDITIONS

- Existing buildings that do not conform to the provisions of this Code may continue in use as they are until a Substantial Modification is requested, at which time a Consolidated Review Committee (CRC) shall determine the provisions of this section that shall apply.
- Existing buildings that when renovated have at any time received a certificate of occupancy shall not require upgrade to the current 2006 International Building Code and may meet the standards of the code under which they were originally permitted.
- The modification of existing buildings is permitted by right if such changes result in greater conformance with the specifications of this section.
- Where buildings exist on adjacent lots, it may be required that a proposed building match one or the other of the adjacent Setbacks and heights rather than the provisions of this Code.
- The restoration or rehabilitation of an existing building shall not require the provision of (a) parking in addition to that existing or (b) on-site stormwater retention/detention in addition to that existing, except to the extent required by applicable state or federal law.

SPECIAL REQUIREMENTS

An Infill Community Plan may designate the following special requirements:

- a. A differentiation of the Thoroughfares as a Primary-Grid (P-Grid) and a Secondary-Grid (S-Grid). Buildings along the P-Grid shall be held to the highest standard of this Code in support of pedestrian activity. Buildings along the S-Grid may be more readily considered for variances allowing automobile-oriented standards.
- b. A designation for recommended retail Frontage requiring that a retail building provide a shopfront at sidewalk level along the entire length of the Frontage. The shopfront shall be no less than 60% glazed in clear glass and provided with an awning overlapping the sidewalk as generally illustrated in Table 7. The first floor shall be confined to retail use through the depth of the First Layer.
- c. A designation of Coordinated Streetscape Frontage, requiring that the Public and Private Frontages be coordinated as a single, coherent landscape and paving design.
- d. A designation of Buildings of Value, requiring that such buildings and structures may be altered or demolished only when in accordance with preservation standards and protocols adopted by the Planning Commission.

Appendix

Form-Based Code of the Sunshine Street Mixed-Use Corridor Overlay District

SPECIAL DISTRICT

Building Disposition

- a. Buildings shall be disposed in relation to the boundaries of their lots.
- b. One Principal Building at the Frontage, and one Outbuilding to the rear of the Principal Building, may be built on each lot.
- c. Lot coverage by building shall not exceed that shown in Section 1.0
- d. Facades shall be built parallel to a rectilinear Principal Frontage Line.
- e. Setbacks for Principal Buildings shall be as shown in Section 1.0. In the case of an Infill lot, Setbacks shall match one or the other of the existing adjacent Setbacks.
- f. Rear Setbacks for Outbuildings shall be as shown in Section 1.0.
- g. Building Types shall be as shown in Table 9.
- h. A minimum Residential housing mix of three Types (none less than 20%) shall be required in the overly district, selected from Table 9.

Building Configuration

- a. Private Frontage types shall conform to and be allocated in accordance with Table 7 and Section 1.0.
- b. Awnings may encroach the public sidewalk without limit. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to 50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.
- c. Loading docks and service areas are not permitted on Frontages.
- d. Building Heights shall conform to Table 8 and be as shown in Section 1.0

Parking Standards

- a. Vehicular parking shall be required as shown in Table 12.
- b. Parking shall be accessed by the Alley or Side street, when such are available.
- c. Parking lots shall be masked from the Frontage by a constructed or landscaped bufferyard.
- d. All parking areas except for Driveways shall be located at the Third Layer as illustrated in Section 1.0, New garages shall also be at the Third Layer.
- e. The required parking may be provided within one-quarter mile of the site that it serves, subject to approval by variance.
- f. A minimum of one bicycle rack place shall be provided within the Public or Private Frontage for every fifteen vehicular parking spaces.

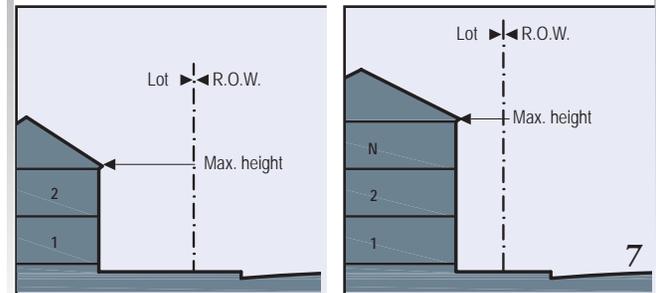
Landscape Standards

- a. A minimum of one tree to match the species of street trees on the Public Frontage shall be planted within the First Layer for each 30 feet of Frontage Line as illustrated in Table 16D.
- b. Trees of species matching the planting on the Public Frontage as shown in Table 4. Lawn shall be permitted.

Building Function & Density

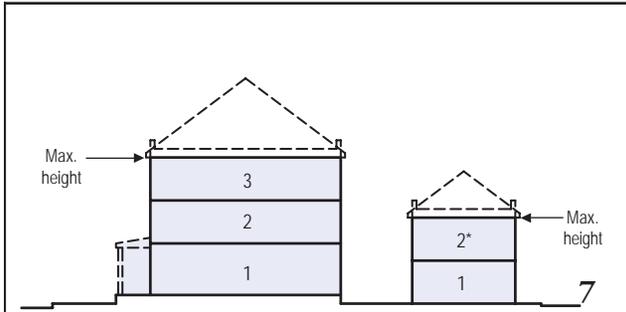
- a. Buildings shall conform to the Functions described in Section 1.0.
- b. The Actual Parking available to meet the Required Parking shown on Table 12 shall constitute the Base Density. Functions shall be limited by the Base Density.
- c. The Base Density may be adjusted upward by adding the Actual Parking available for each of two Functions within any pair of adjacent Blocks, and the resulting sum then multiplied by the corresponding Sharing Factor. The result shall be the Effective Parking available for calculating an Adjusted Density.

TABLE 8: Building Configuration. This table shows prescribed building heights. The vertical extent of a building is measured by number of stories, not including a raised basement or an inhabited attic. Heights are measured from the average grade of the frontage line to the eave of a pitched roof or to the surface of a flat roof.



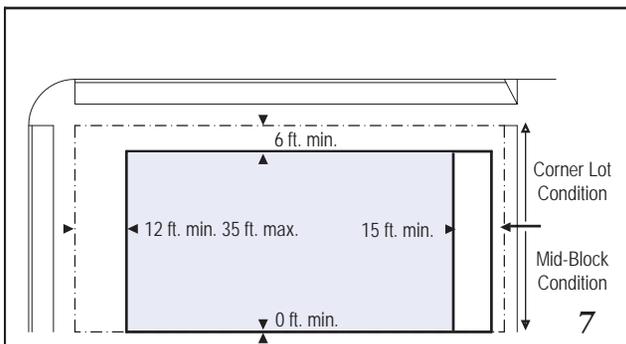
BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown.
2. Buildings shall have facades along principal frontage lines and elevations along lot lines. (see Table 16E).

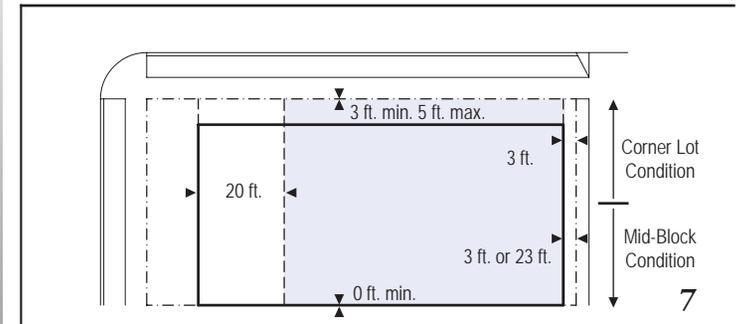


SECTION 1.0

BUILDING FUNCTION	
a. Residential	existing use
b. Lodging	limited use
c. Office	limited use
d. Retail	limited use
BUILDING HEIGHT FOR NEW CONSTRUCTION	
a. Principal Building	3 stories max, 2 min
b. Outbuilding	1 story max.
LOT OCCUPATION	
a. Lot Width	30 ft min, 96 ft max
b. Lot Coverage	60% max
BUILDING TYPE (see Table 9)	
a. Edgeyard	permitted
b. Sideyard	permitted
c. Rearyard	permitted
d. Courtyard	prohibited
BUILDING DISPOSITION	
a. Front Setback	6 ft. min. 18 ft. max.
b. Side Setback	0 ft. combined min.
c. Rear Setback	20 ft. min.*
OUTBUILDING DISPOSITION	
a. Front Setback	20 ft. min. + bldg. setback
b. Side Setback	5 ft. min.
c. Rear Setback	20 ft.
PRIVATE FRONTAGES (see Table 7)	
a. Common Lawn	prohibited
b. Porch & Fence	permitted
c. Terrace or L.C.	permitted
d. Forecourt	prohibited
e. Stoop	permitted
f. Shopfront & Awning	permitted
g. Gallery	prohibited
h. Arcade	prohibited
PARKING PROVISIONS	
See Table 12	

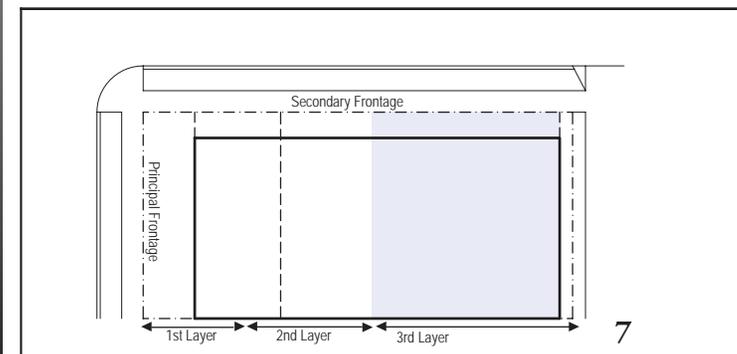
OUTBUILDING PLACEMENT

1. The elevations of the out buildings shall be distances from the lot lines as shown.



PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 16D).
2. Trash containers shall be stored within the 3rd Layer.



Appendix

Form-Based SmartCode of the Sunshine Overlay District

Architectural Standards

- a. Building wall materials may be combined on each Facade only horizontally, with the heavier below the lighter.
- b. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building Facade. The Streetscreen may be replaced by a hedge or fence. Streetscreens shall have openings no larger than necessary to allow automobile and pedestrian access.
- c. All openings, including porches, galleries, and windows, with the exception of storefronts, shall be square or vertical in proportion.
- d. Openings above the first Story shall not exceed 50% of the total building wall area, with each Facade being calculated independently.
- e. Doors and windows that operate as sliders are prohibited along Frontages.
- f. Pitched roofs, if provided, shall be symmetrically sloped no less than 5:12, except that porches and attached sheds may be no less than 2:12
- g. Flat roofs shall be enclosed by parapets a minimum of 42 inches high, or as required to conceal mechanical equipment.
- h. The exterior finish material on all new and remodeled Facades shall be limited to brick, stone, wood siding, fiber-cement siding and/or stucco.
- i. Balconies and porches shall be made of painted wood or metal.
- j. Fences, if provided within the First Lot Layer shall be painted. Fences at other Layers may be of wood board or chain link.

Environmental Standards

- a. The species of landscape installed shall consist primarily of durable species tolerant of soil compaction.
- c. Impermeable surface shall be confined to the ratio of lot coverage by building.
- d. To the extent not inconsistent with applicable state or federal law, management of storm water shall be primarily off-site through underground storm drainage and there shall be no retention or detention required on the individual lot.

Visitability Standards

- a. There shall be provided one zero-step entrance to each building from an accessible path at the front, side, or rear of each building.
- b. All first floor interior doors (including bathrooms) shall provide 32 inches of clear passage.
- c. There shall be a half or full bath provided on the first Story of each building.

Signage Standards

- a. One address number no more than 6 inches measured vertically shall be attached to the building in proximity to the principal entrance or at a mailbox.
- b. One sign for each business may be permanently installed perpendicular to the Facade.
- c. Other signage shall be permitted in accordance with the standards and procedures of the Sunshine Street Mixed-Use Overlay District.

Ambient Standards

- a. Sound levels measured at the building Frontage shall not exceed 65 decibels from sunrise to sunset and 55 decibels from sunset to sunrise.
- b. Average lighting levels measured at the building Frontage shall not exceed 2.0 fc (foot-candles) .
- c. Streetlights shall be of a general type.
- d. Outdoor storage shall be screened from view from any Frontage by a Streetscreen.

TABLE 9: Building Type. This table approximates the location of the structure relative to the boundaries of each individual lot, establishing suitable basic building types.

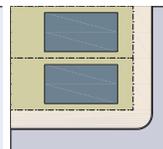
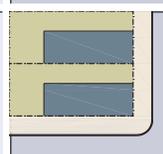
<p>a. Edgeyard: Specific Types - Single family House, Cottage, Villa, Estate House, Urban Villa. A building that occupies the center of its lot with Setbacks on all sides. This is the least urban of types as the front yard sets it back from the frontage, while the side yards weaken the spatial definition of the public Thoroughfare space. The front yard is intended to be visually continuous with the yards of adjacent buildings. The rear yard can be secured for privacy by fences and a well-placed Backbuilding and/or Outbuilding.</p>	
<p>b. Sideyard: Specific Types - Charleston Single House, zero-lot-line house. A building that occupies one side of the lot with the Setback to the other side. The visual opening of the side yard on the street frontage causes this building type to appear freestanding. A shallow frontage Setback defines a more urban condition. If the adjacent building is similar with a blank party wall, the yard can be quite private. This type permits systematic climatic orientation in response to the sun or the breeze.</p>	
<p>c. Rearyard: Specific Types - Townhouse, Rowhouse, Live-Work unit, perimeter block. A building that occupies the full frontage, leaving the rear of the lot as the sole yard. This is a very urban type as the continuous Facade steadily defines the public Thoroughfare. The rear Elevations may be articulated for functional purposes. In its Residential form, this type is the Rowhouse. For its Commercial form, the rear yard can accommodate substantial parking.</p>	

Table 3C: Thoroughfare Assembly. This thoroughfare is assembled from the elements that incorporate the Public Frontages of Table 4B. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width.

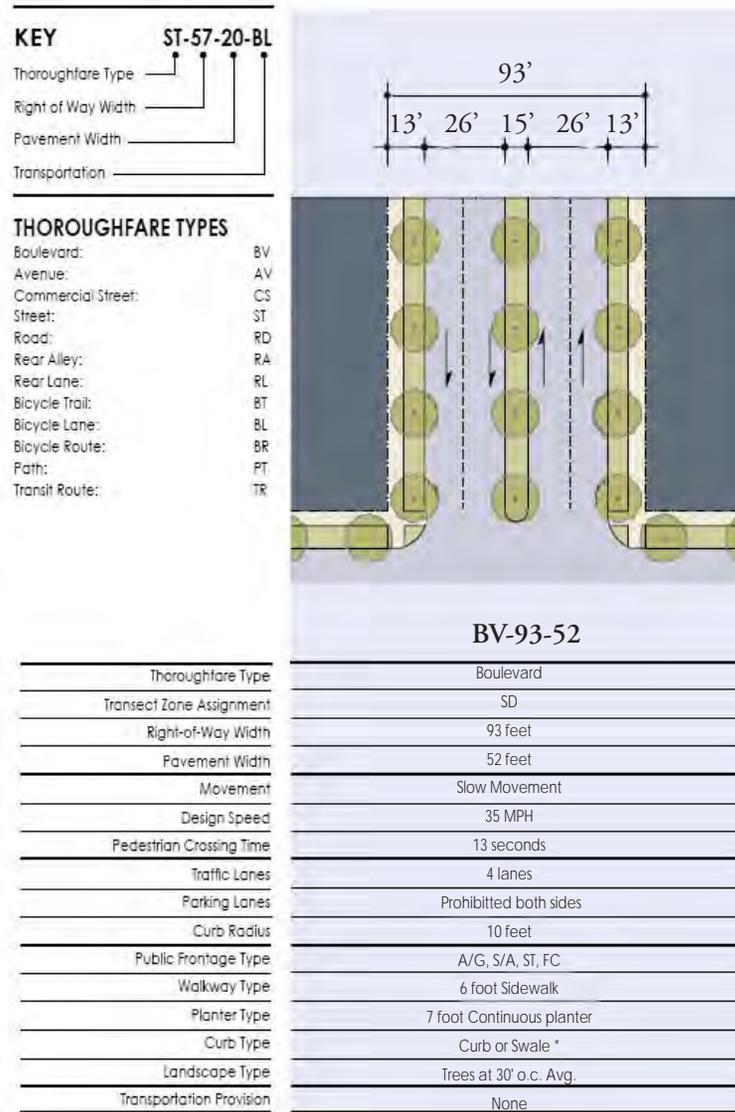


Table 4B: Public Frontages - Specific. This table assembles prescriptions and dimensions for the public frontage elements - curbs, walkways and planters – relative to specific thoroughfare types.

Public Frontage Type	RR & SR	RS	RS-SS-AV	RS-CS-AV-BV
<p>a. Assembly: The principal variables are the type and dimension of curbs, walkways, planters and landscape.</p> <p>Total Width</p>	12-24 feet	12-18 feet	12-18 feet	18-24 feet
<p>b. Curb: The detailing of the edge of the vehicular pavement, incorporating drainage.</p> <p>Type</p> <p>Radius</p>	Open Swale 10-30 feet	Raised Curb 5-20 feet	Raised Curb 5-20 feet	Raised Curb 5-20 feet
<p>c. Walkway: The pavement dedicated exclusively to pedestrian activity.</p> <p>Type</p> <p>Width</p>	Path 4-8 feet	Sidewalk 4-8 feet	Sidewalk 4-8 feet	Sidewalk 12-20 feet
<p>d. Planter: The layer which accommodates street trees and other landscape.</p> <p>Arrangement</p> <p>Species</p> <p>Planter Type</p> <p>Planter Width</p>	Clustered Multiple Continuous Swale 8 feet-16 feet	Regular Alternating Continuous Planter 8 feet-12 feet	Regular Single Continuous Planter 8 feet-12 feet	Regular Single Continuous Planter 4 feet-6 feet
<p>e. Landscape: The recommended plant species.</p> <p>Trees</p> <p>Understory</p>				

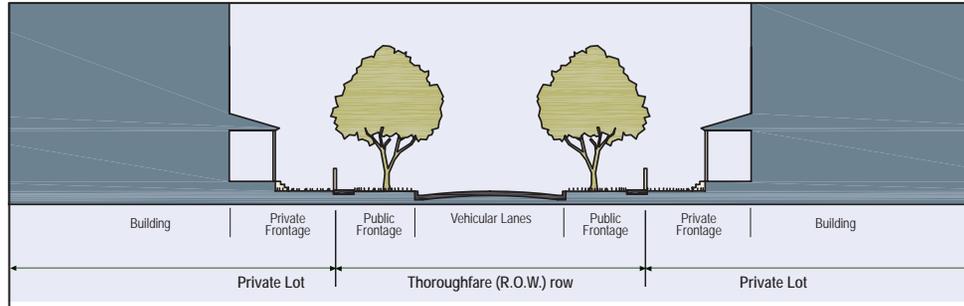
Appendix

Form-Based SmartCode of the Sunshine Overlay District

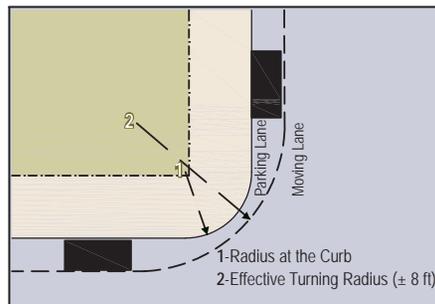
Tables 7 & 16

Table 16: Definitions Illustrated

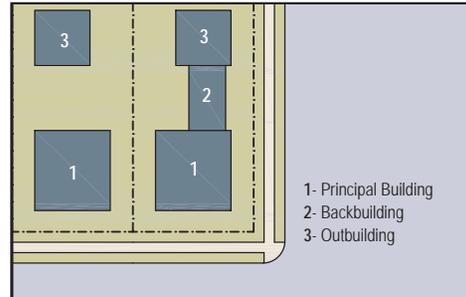
a. THOROUGHFARE & FRONTAGES



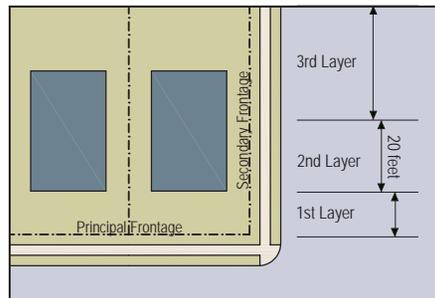
b. TURNING RADIUS



c. BUILDING DISPOSITION



d. LOT LAYERS



e. FRONTAGE & LOT LINES

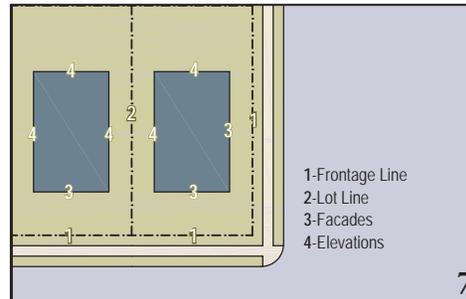
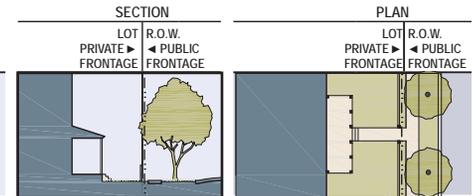
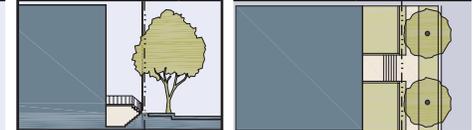


TABLE 7: Private Frontages. The Private Frontage is the area between the building and the lot lines.

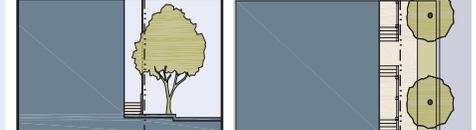
a. Porch & Fence: a frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroaching. A fence at the frontage line maintains the demarcation of the yard. The porches shall be no less than 8 feet deep.



b. Terrace or Light Court: a frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken light court. This type buffers residential use from urban sidewalks and removes the private yard from public encroachment. The terrace is suitable for conversion to outdoor cafes.



c. Stoop: a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.



d. Shopfront and Awning: a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to the maximum extent possible.

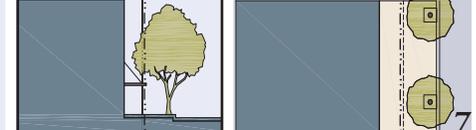


TABLE 12: Parking Calculation. The Required Parking table summarizes the parking requirements of typical building functions, for each site or, conversely, the amount of building allowed on each site given the parking available.

REQUIRED PARKING		SHARING FACTOR			
		Function	with	Function	Function
RESIDENTIAL	2.0 / dwelling	RESIDENTIAL	1	RESIDENTIAL	RESIDENTIAL
LODGING	1.0 / bedroom	LODGING	1.1	LODGING	LODGING
OFFICE	1.5 / 1000 sq. ft.	OFFICE	1.1	OFFICE	OFFICE
RETAIL	2.0 / 1000 sq. ft.	RETAIL	1.4	RETAIL	RETAIL
OTHER	To be determined				

TABLE 5: Public Lighting. Lighting varies in brightness and also in the character of the fixture according to the Transect. The table shows four common types. A listed set of streetlights corresponding to these types would be approved by the utility company and listed on the page.

SD	Specifications
	<input type="checkbox"/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
	<input type="checkbox"/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
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TABLE 6: Public Planting. This table shows five common types of street tree shapes and their appropriateness within the Special District. The local planning office selects species appropriate for the bioregion.

SD	Specific Planting
	<input type="checkbox"/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
	<input type="checkbox"/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>
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Table 15: Special District Summary. Special Districts (SD) are areas that cannot comply with this Code. The metrics for each column of this table (SD1, SD2, etc.) are to be filled out with the details of each Special District as they currently exist, or as they are permitted.

	DISTRICT SD1	DISTRICT SD2
A. ALLOCATION OF ZONES		
a. TND / Village	X	
B. BASE DENSITY ALLOCATION		
a. HousingBy Right	X unit/X ac. avg	
b. By TDR	X	
c. Other Functions	X	
C. BLOCK SIZE		
a. Block Perimeter	X	
D. PUBLIC FRONTAGES		
a. Residential Street	X	
b. Standard Street	X	
c. Commercial Street	X	
d. Avenue	X	
e. Boulevard	X	
f. Rear Lane	X	
g. Rear Alley	X	
E. CIVIC SPACES		
a. Park	X	
b. Green	X	
F. LOT OCCUPATION		
a. Lot Width	X	
b. Lot Coverage	X	
G. BUILDING SETBACK		
a. Front Setback	X	
b. Side Setback	X	
c. Rear Setback	X	
H. BUILDING TYPE		
a. Edgeyard	X	
b. Sideyard	X	
c. Rearyard	X	
I. PRIVATE FRONTAGES TYPE		
a. Porch & Fence	X	
b. Terrace or L.C.	X	
c. Stoop	X	
d. Shopfront	X	
e. Parking Lot	X	
J. BUILDING HEIGHT		
a. Principal Building	X	
b. Outbuilding	X	
K. BUILDING FUNCTION		
a. Residential	X	
b. Lodging	X	
c. Office	X	
d. Retail	X	

Note: This table to be completed as each Special District is approved.

Appendix

Funding Sources

In order to facilitate the multitude of enhancements that are possible along the Sunshine Street corridor there are a couple of sources of revenue that the residents of the neighborhood can tap into. These funds allow for the creation and maintenance of public facilities that benefit the public. Examples of specific ideas that can benefit the area include:

- the acquisition of property along Sunshine Street to create community green spaces in vacant lots or renovate an existing residence for use as a neighborhood association office
- widening, adding texture, and maintaining the sidewalks along Sunshine Street
- creating and maintaining a landscaped buffer between the street and the sidewalk to foster a more pedestrian friendly environment
- adding decorative, pedestrian scale lighting to foster a safer nighttime environment
- creating textured crosswalks with refuge islands to allow more opportunities to cross Sunshine between National and Kimbrough
- using sustainable practices to create off-street parking for businesses



Neighborhood Improvement District (NID)

A Neighborhood Improvement District finances improvements to be used by the public and must confer a benefit on property within the district. A NID can be formed by an election of the voters residing in the NID boundary or by a petition of the owners of record of at least two-thirds by area of all real property located in the proposed district. In this instance the petition is filed with the City and the City then creates the NID by ordinance.

- Revenues- Special assessment on property located in the NID District.
- Eligible Costs- Facilities used by the public that benefit the district; for instance, property acquisition, streets, sidewalks, storm and sanitary systems, and service connections from utility mains, conduits and pipes parks, streetlights, and off-street parking.
- Establishment- Either by petition of the property owners or by majority vote of registered voters in the district. City Council then has discretion to vote to establish the district.
- Governance- Since the petition sets forth the specific improvements to be financed through the district, no governing board is required.
- Life of District- 20 years.
- Example: Parkwood Neighborhood Improvement District (park acquisition, improvements, and maintenance), Lone Pine Green Space NID (parkland/open space)

Responsibilities And Challenges

Public hearings concerning the specifics of the project, its costs, and other specific information pertinent to the project must be conducted prior to commencement of work on any project of the NID so that any written or oral objections may be considered.

The ability of Missouri's neighborhoods and communities to join together for the purpose of improving their public use facilities for the enjoyment, convenience, and safety of all citizens is an outstanding example of local economic development excellence. The Missouri Department of Economic Development has the information, tools and professional expertise to assist Missouri communities create better neighborhoods.

Community Improvement District (CID)

A CID is either a political subdivision or a not-for-profit corporation organized for the purpose of financing a wide range of public use facilities and establishing and managing policies and public services relative to the needs of the district. It is created when a petition is filed with the city by property owners owning at least 50% of the assessed value of the real property, and more than 50% per capita of all owners of real property within the proposed CID. The city establishes a CID by ordinance, but then it is a separate legal entity that is distinct and apart from the municipality. Any responsibilities or organizational ties to the city must be set forth in the charter ordinance.

- Revenues- include imposed property tax, sales tax, or special assessments.
- Eligible Costs- New facilities or improvements to existing facilities that are used by the public; for instance, convention centers, streetscapes, parking lots, murals, fountains, parks, sidewalks, streets, traffic signalization, utilities, storm and sewer systems.
- Establishment- Requires development of a 5-year business plan and petition setting forth assessment method and services to be delivered. Property owners representing over 50% of the property owned groups and over 50% of the total assessed value in the district must sign the petition. City Council votes to establish the district. Significant notice and public hearings are required.
- Governance- CID is governed by a board of directors comprised of property owners, business owners, and/or residents of the district as set forth in the petition.
- Life of District- Established in petition. CID's designed to provide services are typically 5-10 years; public infrastructure typically 20-30 years.
- Example: Downtown Springfield. CID used for the beautification of streetscapes, maintenance of public parking lots, as well as cooperative marketing efforts.

Appendix

Community Involvement & Resources

Community Members in attendance at the meetings

Rose Barber	620 E. University Street
Robert & Elizabeth Cartwright	1840 S. Hampton Avenue
Edward Castillo	711 E. Sunshine Street
	701 E. Sunshine Street
	635 E. Sunshine Street
David & Ann Catlin	610 E. University Street
Mark Cooper	635 E. University Street
Brenda Cummings	1039 E. University Street
Randall Doennig	1720 S. Kimbrough
Rita Fabin	700 E. Kingsbury
Brian Fogle	1125 E. Kingsbury
Martha Gaither	703 E. Stanford
Mary Harper	1100 E. University Street
Claudia & Keith Hartner	1851 S. Holland Avenue
Jeff Hefner	
Kim & Dwight Hughes	1033 E. Sunshine Street
Linda Hunt	1014 E. Sunshine Street
Stella Lee	620 E. Sunshine Street
Tim Montgomery	1046 E. Sunshine Street
Mike O'Brien	1031 E. University Street
Kevin Parnell	927 E. University Street
Linda Regan	901 E. University Street
John Ryan	1047 E. Sunshine Street
Helen & Bill Smith	1739 S. National Avenue
Julie Smith	
Rick Stephenson	1001 E. Sunshine Street
Don Thomson	1029 E. University Street
Chuck & Sheila Wentz	959 E. University Street
Karen & David Yancy	2516 E. Barataria (home)
	1054 E. Sunshine St. (rental)



11



11

Community Resources:

There are many resources available to learn more about the topics discussed in this booklet. Some of the ones we thought would be helpful are as follows:

Books

- “The Smartcode Solution to Sprawl” by Chad Emerson. © 2007
- “Smart Growth Zoning Codes” by Steve Tracy. © 2003
- “Codifying New Urbanism: How to Reform Municipal Land Development Regulations” by Congress for the New Urbanism. © 2004

Magazines

- Places, Forum of Design for the Public Realm. “Building Communities Across the Transect” Issue 18.1 Spring 2006.

Websites

- www.smartcodecomplete.com
- **SmartCode Version 6.5 Spring 2005: A comprehensive Form-based planning ordinance** - www.tndtownpaper.com/images/SmartCode6.5.pdf
- **SmartCode Version 8.0** - www.placemakers.com/smartcode/3000-00-Clean_SmartCodev8.0.pdf
- **Springfield Zoning Ordinances** - www.ci.springfield.mo.us/egov/planning_development/zoning/pdf_files/zoningord.pdf
- **Rental property maintenance code example from the city of Westminster, Colorado** - www.ci.westminster.co.us/Code/Title11/T11C12.htm
- **Report on ways to address rental property maintenance from the Kansas City area** - www.marc.org/firstsuburbs/RegofRentalProperty.pdf
- **Congress for the New Urbanism website** - www.cnu.org/
- **Getting to Smart Growth: 100 Policies for Implementation** - www.smartgrowth.org/pdf/gettosg.pdf
- **Getting to Smart Growth II: 100 More Policies for Implementation** - www.smartgrowth.org/pdf/gettosg2.pdf



Resources & References

- Works Cited
- Contact Information

Sunshine Street Corridor



Resources and References

Works Cited

Books

Burdern, Ernest. Entourage. McGraw-Hill Inc., New York, 1991.

Calthorpe, Peter and Fulton, William. The Regional City: planning for the end of sprawl. Island Press, Washington, 2001.

Congress for the New Urbanism. Codifying New Urbanism: How to Reform Municipal Land Development Regulations. American Planning Association, Chicago, 2004.

Gindroz, Ray and Robinson, Rob, et al. The Architectural Pattern Book. W.W. Norton & Company, New York & London, 2004

Schwanke, Dean, et al. Mixed-Use Development Handbook. Second edition. ULI- the Urban Land Institute, Washington, D.C., 2003

Sucher, David. City Comforts: How to Build an Urban Village. City Comforts Inc., Seattle, 2003.

Untermann, Richard K. Accommodating the Pedestrian. Van Nostrand Reinhold Company, New York, 1984.

Magazines

Places, Forum of Design for the Public Realm. "Building Communities Across the Transect" Issue 18.1
Spring 2006.

City Documents

Abrocultural Design Guidelines. Springfield 1997.

City of Springfield Zoning Ordinance. Last Amended September 26, 2006

Ozarks Transportation Organization. Congestion Management System Phase II: Identification of Congested Corridors and Mitigation Strategies. Draft 11/05

Ozarks Transportation Organization. Journey 2030: Long Range Transportation Plan. Adopted April 2006.

Phelps Grove Neighborhood Plan. Department of Planning and Development. Adopted February 18, 1997.

Roundtree Urban Conservation District. Last amended May 29, 2005.

Walnut Street-West Urban Conservation District. Last amended 2000.

Websites

www.smartcode.org

www.cnu.org

www.lgc.org

www.charrettecenter.net

www.brooksidekc.org

www.brooksideshops.com

www.newurbannews.com

www.tndtownpaper.com/images/SmartCode6.5.pdf

www.warrensburg-mo.com/Community_Development/CD_Brochure/Innovative_Financing_Brochure.pdf

www.missouridevelopment.org/Community%20Services/Local%20Finance%20Initiatives.aspx

Resources and References

Works Cited-Picture Documentation

Number on picture corresponds with sources below.

Example:



This image was found on
www.google.com

1. Image from www.google.com
2. Image from Smart Growth Software
3. Image from www.brooksidekc.org
4. Image from Accommodating the Pedestrian
5. Image from www.ci.kirkwood.mo.us/
6. Image from www.businessdecision.info.www.coolcat.org/login.aspx
7. Image from Smart Code (Version 6.5 and 8.0)
8. Image from www.ci.springfield.mo.us/index.html
9. Photograph taken by Christine Friederich
10. Photograph taken by Lannette Guerra
11. Photograph taken by Audrey McNamara
12. Photograph taken by Carly Rickerson
13. Photograph taken by Kelsey Stein

14. Drawing by Christine Friederich
15. Drawing by Lannette Guerra
16. Drawing by Audrey McNamara
17. Drawing by Carly Rickerson
18. Drawing by Kelsey Stein
19. www.samoanet.com
20. Santana Row
21. www.sftours.com
22. www.mougalian.com
23. www.cyburbia.org/gallery/showphoto.php?photo=29
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